





UPTOWN WHITTIER STREETSCAPE BEAUTIFICATION PLAN

TABLE OF CONTENTS



- 1.1 Project Background
- 1.2 Project Site
- 1.3 Positioning Uptown

02. EXISTING ANALYSIS

- 2.1 Regional Case Studies
- 2.2 Existing Site Conditions

03. STREETSCAPE PLAN

- 3.1 Guiding Principles
- 3.2 Streetscape Beautification Plan
- 3.3 Streescape Families
- 3.4 Key Nodes

04. STREETSCAPE PALETTE

- 4.1 Paving
- 4.2 Curbs
- 4.3 Street Trees
- 4.4 Understory Planting
- 4.5 Site Furnishings
- 4.6 Lighting
- 4.7 Installations
- 4.8 Accessibility + Loading

your UPTOWN PEOPLE, PLACES, STREETS,

05. PHASE 1 STUDY AREA

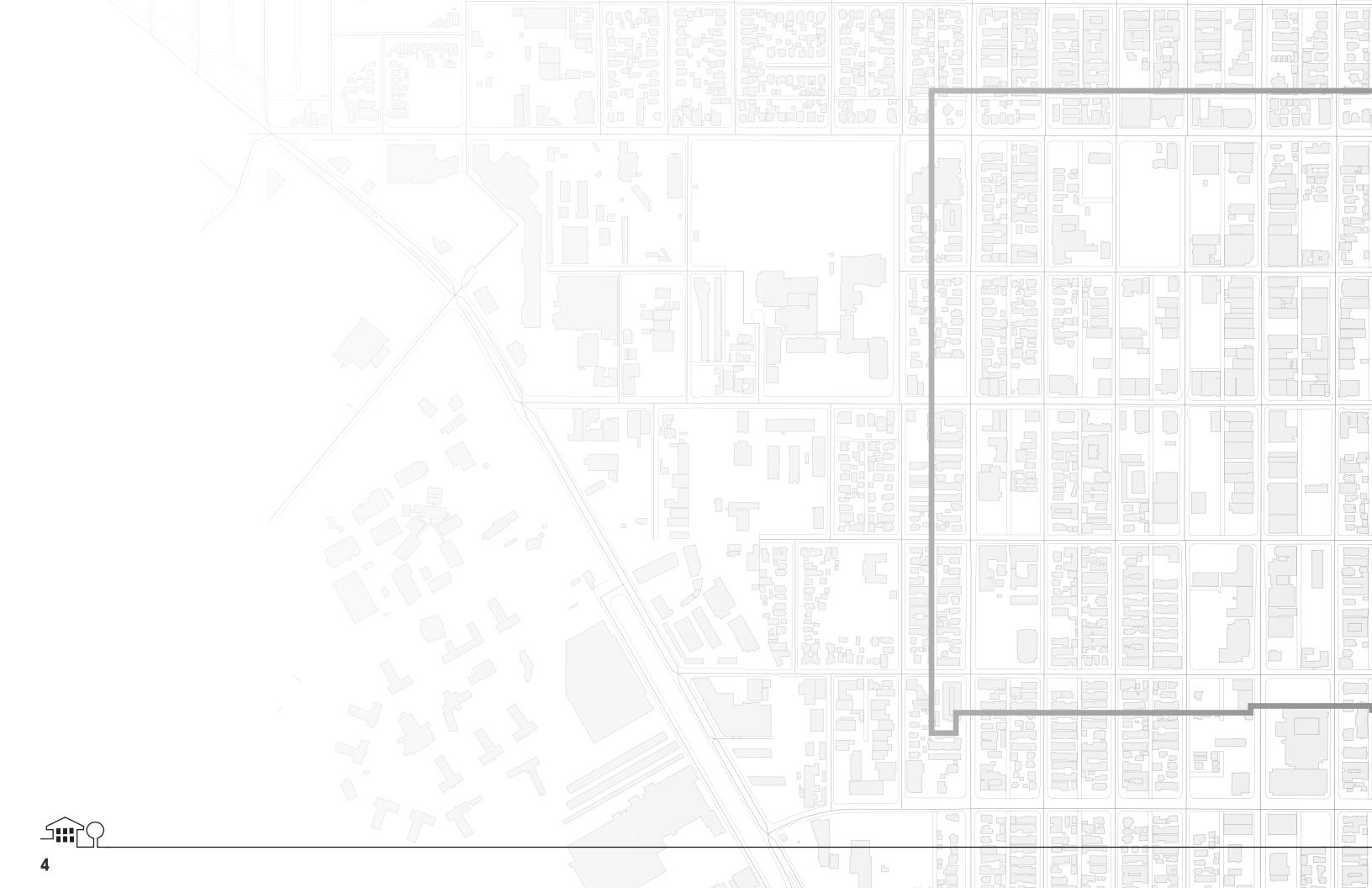
- 5.1 Overview
- 5.2 Gardens of Uptown Concept Plan

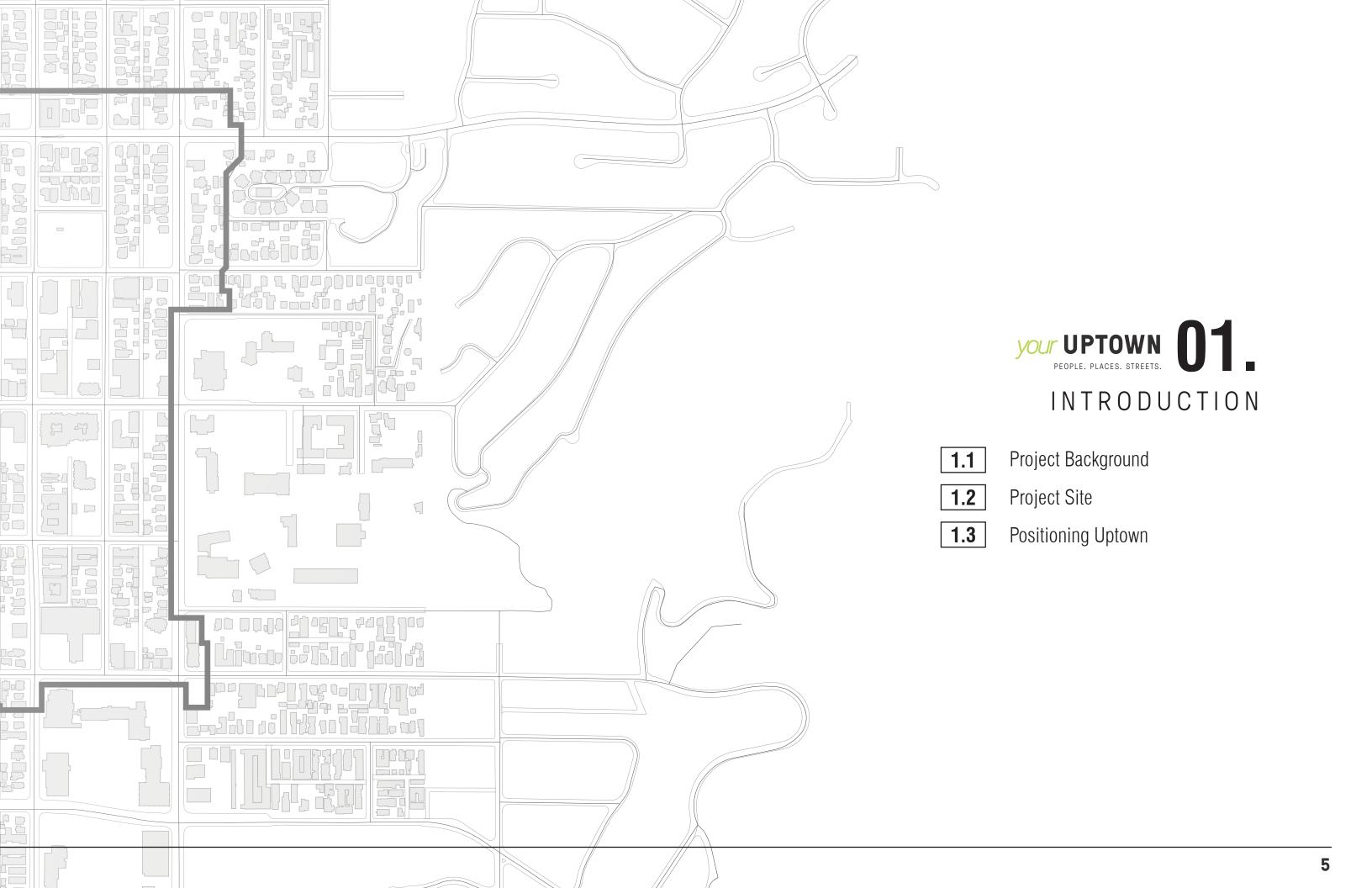
06. COMMUNITY INVOLVEMENT OVERVIEW

- 6.1 Workshop #1
- 6.2 Workshop #2
- 6.3 Workshop #3

07. APPENDIX

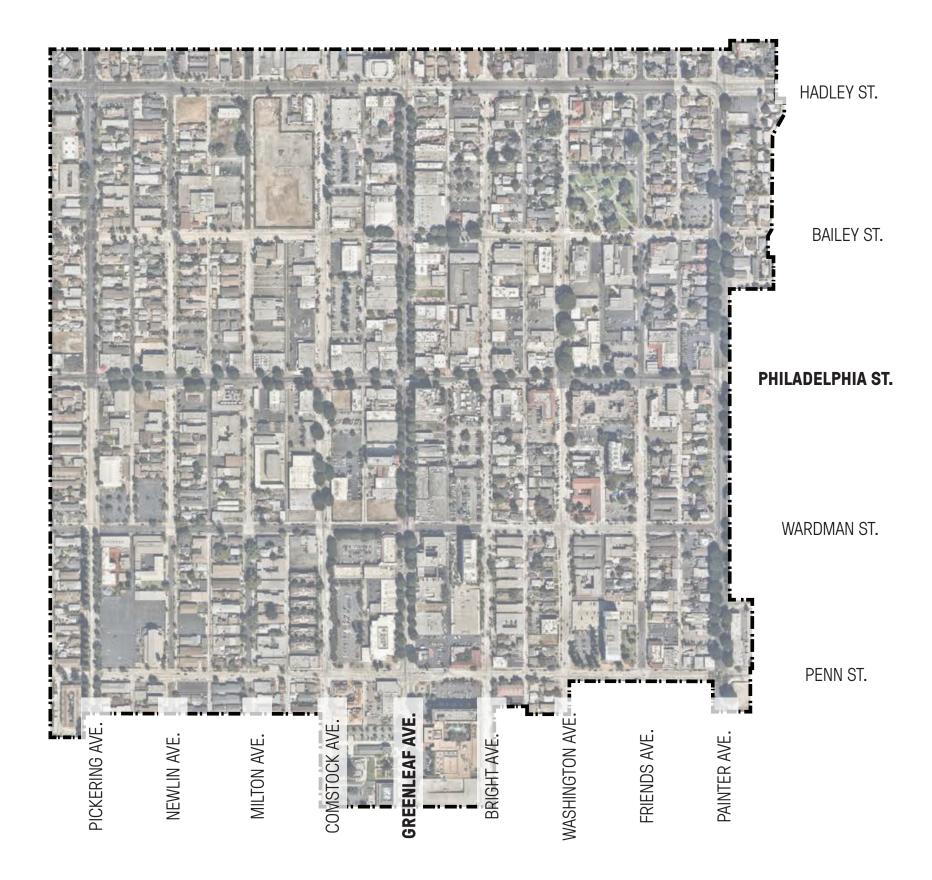
- 7.1 Phase 1 Streetscape Plan Alternative
- 7.2 Opinion of Probable Cost





STUDY AREA

PROJECT Study Area





WHY?



A STREETSCAPE BEAUTIFICATION PLAN?

WHY

Uptown Whittier, the historic central business district, has the potential of becoming an even brighter jewel in Whittier, as it is a source of pride for its citizens, has a sense of identity from its historic roots, and is a place of great character and attraction due to its unique building form, diversity of activities, locally owned storefronts, restaurants and civic services.

The City's goal is to leverage these assets and shape the area into a destination with a vibrant, urban experience for all. This plan is a result of the direction given in the Uptown Whittier Specific Plan (UWSP).

CELEBRATE THE HISTORIC CORE OF WHITTIER



KEEP UPTOWN COMPETITIVE

05.





ENRICH THE COMMUNITY'S PUBLIC REALM



PROVIDE FOR SAFETY + ACCESSIBILITY

PROMOTE A WALKABLE, ENJOYABLE UPTOWN

PROJECT BACKGROUND

WHAT?

WHAT IS

A STREETSCAPE BEAUTIFICATION PLAN?

The Uptown Whittier Streetscape Beautification Plan process, launched by the Whittier City Council in response to the UWSP, provides design solutions for enhancing the streetscapes of Uptown Whittier in conjunction with the Community Benefit District (CBD), a proposed new parking structure and other infrastructure improvements.

The Plan explores how Uptown streetscapes can be redesigned to best serve and enrich the community of Whittier, the CBD, and how it can celebrate Whittier's rich heritage and environment while keeping pace with changing conditions and trends.

The Plan provides guidelines on:

- Public realm programming
- Design strategies for landscape + hardscape improvements
- Design strategies for street furnishings + pedestrian safety
- Project cost estimation + phasing of implementation

01.

Phasing Of Implementation

immediate

1 month - 1 year

- Approve Uptown Whittier Streetscape Beautification Plan
- Complete Working Drawings for Phase 1 Study Area

P_0

short term

1 year - 5 years

- Construction of One Block Study Area
- CBD Partnership for Street Furnishings and Parklets

 (P_1)

medium term

5 years - 10 years

 Development Built Improvements

P_2

long term

10 years -20 years

- Development Built Improvements
- In-Lieu Fee Improvements

P_3





03.

Public Realm Programming





Results From Community-Based Input



DESIGN STRATEGIES FOR LANDSCAPE + HARDSCAPE ENHANCEMENTS

DESIGN STRATEGIES FOR STREET FURNISHINGS + PEDESTRIAN SAFE AMENITIES

WHERE?

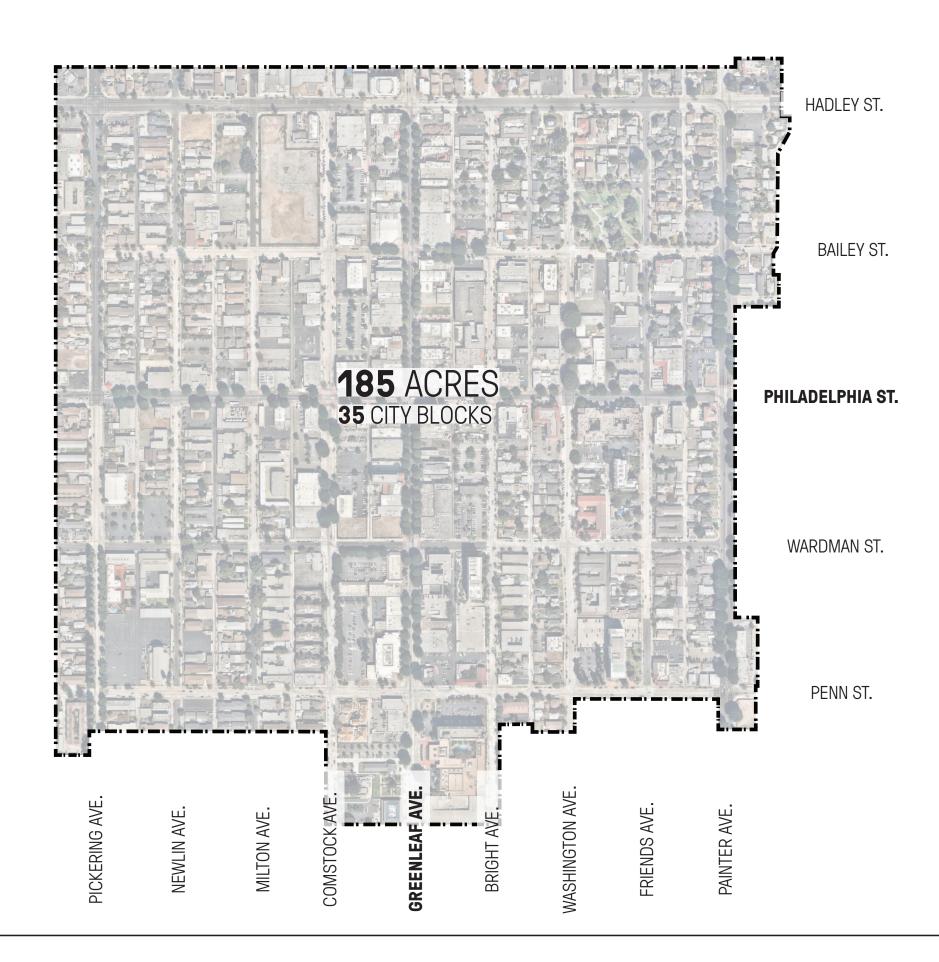


WHERE IS

THE STREETSCAPE BEAUTIFICATION PLAN?

The Uptown Whittier Specific Plan, a document with a strong connection to both the town's history and culture, is the foundation from which this streetscape beautification plan is built. Bounded by Hadley Street, Pickering Avenue, Painter Avenue, and Mar Vista Street, the Uptown region includes a strong commercial core, residential neighborhoods, city and institutional zones, and one primary neighborhood park. The primary focus within this larger region exists along Greenleaf Ave and Philadelphia Street.

This document sets standards and guidelines for the street Right-of-Way (ROW), the area of public land between private parcels that contains the street, sidewalks and parkways.



DEMOGRAPHICS

68.2% Hispanic 24.3% White

4.6% Asian American
1.2% African American
1.7% Others/Multiracial

MAJOR INDUSTRIES

AVERAGE HOME VALUE

\$566,900 Whittier, CA \$547,400 California

MAJOR CONNECTORS

Freeways.....I-605

State Route 72

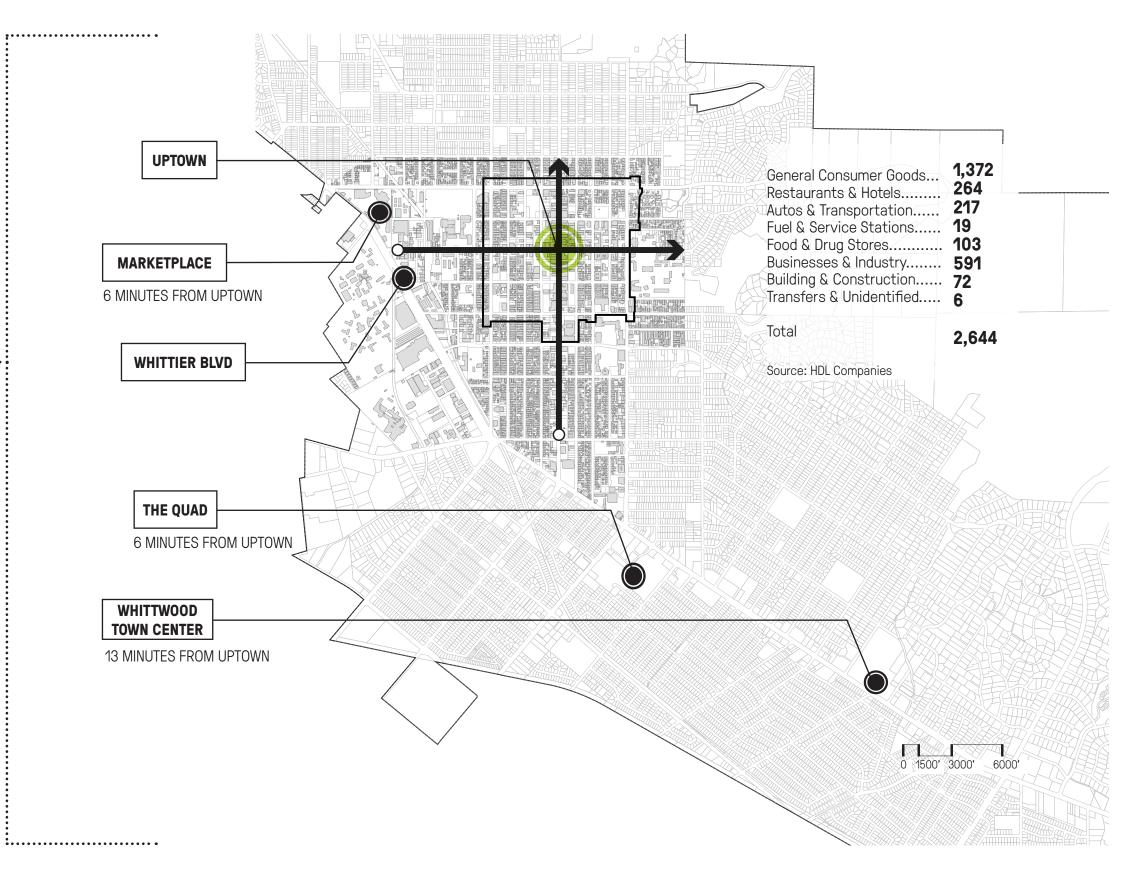
Bus Routes Foothill Transit

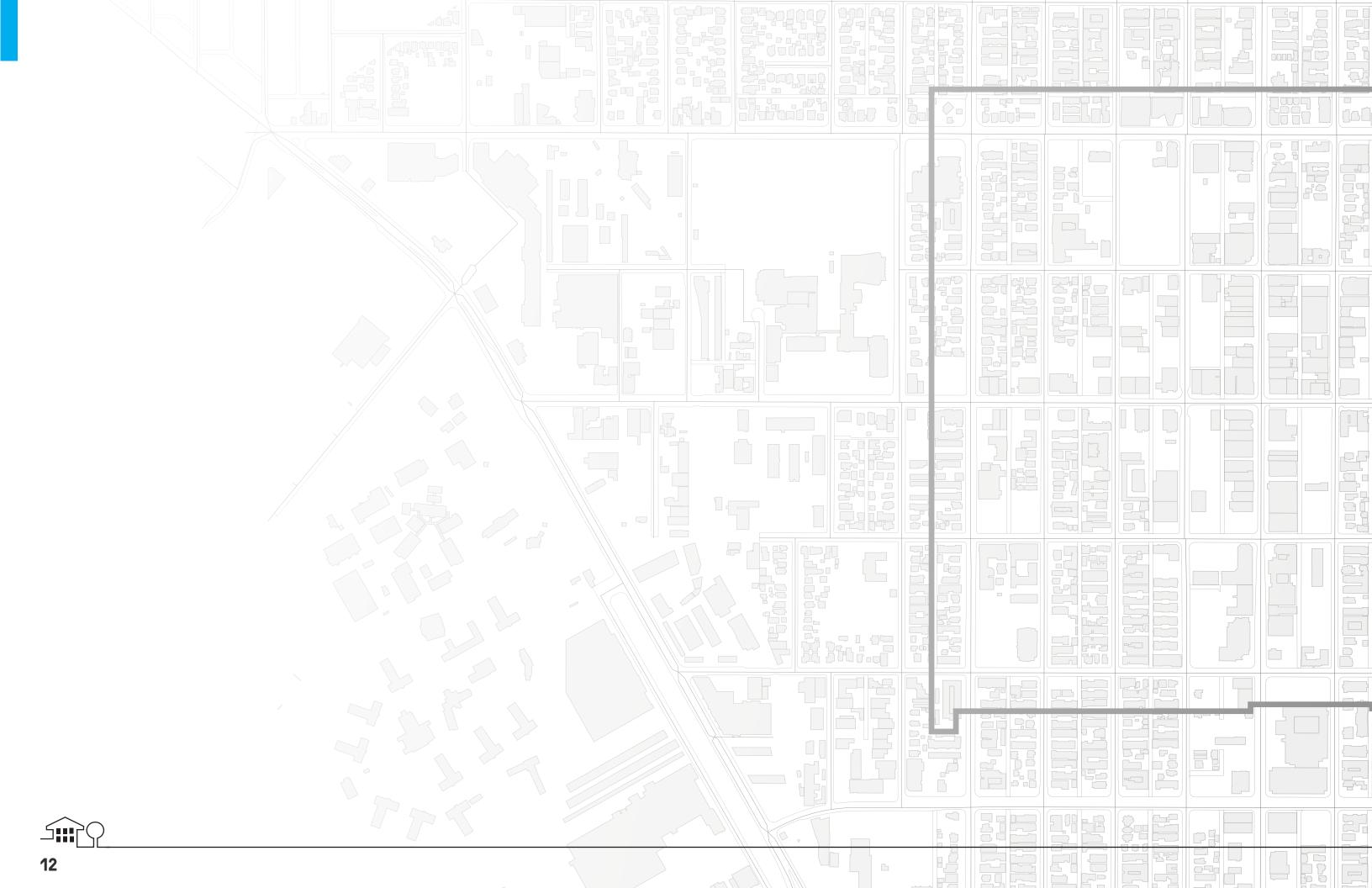
Norwalk Transit

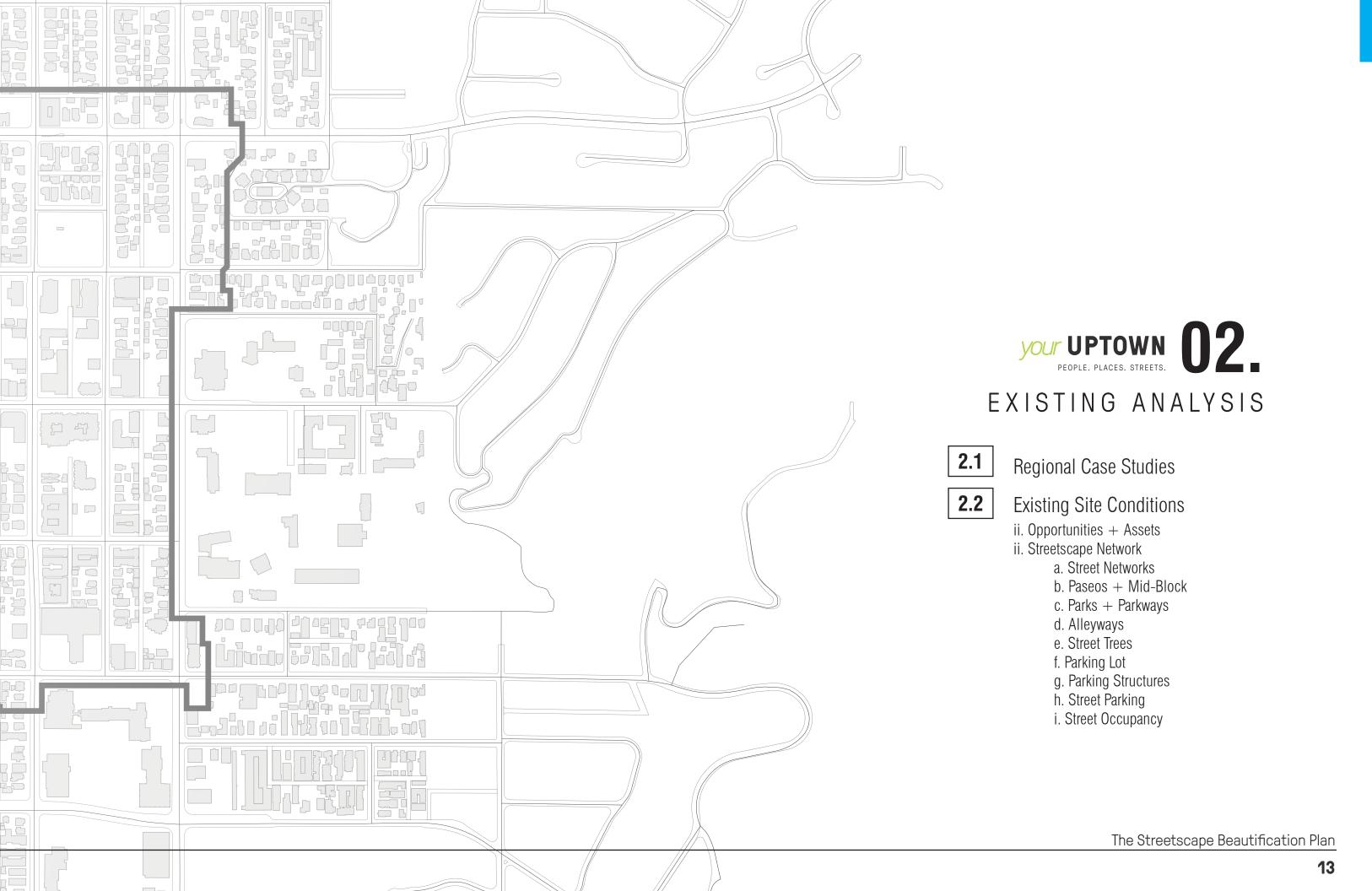
Montebello Line

Metro

AREA 9385 ACRES







HISTORIC SO-CAL DOWNTOWNS

REGIONAL CASE STUDIES



Studying surrounding downtown retail centers, began to give insight on how to learn from and implement design solutions within these regions while also understanding how to better differentiate Uptown Whittier from these commercial zones.



ATTRIBUTES

- Informal landscape
- Curb bump-outs
- Outdoor dining along alleyways
- Paseos & courtyard spaces
- Residential / condominium housing
- Entertainment venues
- Connection to metrolink station



downtown

CLAREMONT

- Village scale retail

- 150 Shops, restaurants & galleries



downtown

PASADENA

ATTRIBUTES

- Streets & courtyard spaces
- Retail alleys / paseos
- All block crosswalks
- Cultural institutions / museums
- Kid space children's museum
- 200+ Boutique / national retailers
- 100+ Restaurants / cafes
- Commercial + residential space











HISTORIC SO-CAL DOWNTOWNS



downtown

BREA

ATTRIBUTES

- Outdoor dining & cafe space
- Great store frontage & facades
- Pedestrian focused district
- Shopping
- Entertainment
- Office Suites
- Live / Work Lofts + Homes
- Holiday + Seasonal Events



downtown

FULLERTON

ATTRIBUTES

- Similar historic character to Uptown
- Similar building heights & store frontages to Uptown
- Connection to Metrolink / Amtrak
- 2,500 Free Public Parking Spaces
- Dining
- Live Music
- Entertainment venues
- Farmers / Craft Markets



downtown

ORANGE

ATTRIBUTES

- Central gathering space that acts as an anchor
- Strong tenant focus of antique stores and boutique shopping
- Strong cafe anchor
- Historic Orange Plaza
- Heart of the City of Orange
- Antique Shops
- Gourmet Eateries





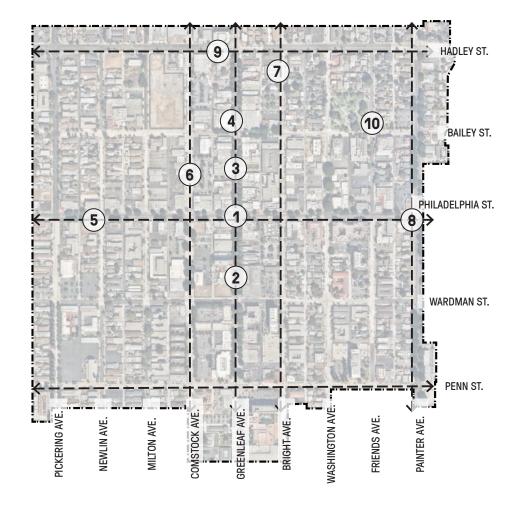








PROJECT SITE OBSERVATIONS



Project Site Observations help identify opportunities and challenges within the study area. The following photos of streetscapes in Uptown Whittier identify opportunities in green boxes and challenges in white boxes.

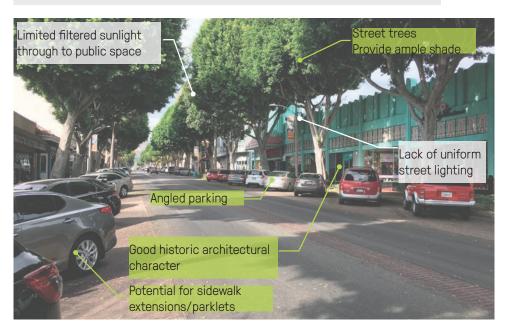
PHILADELPHIA ST./GREENLEAF AVE.



GREENLEAF AVE.



GREENLEAF AVE.



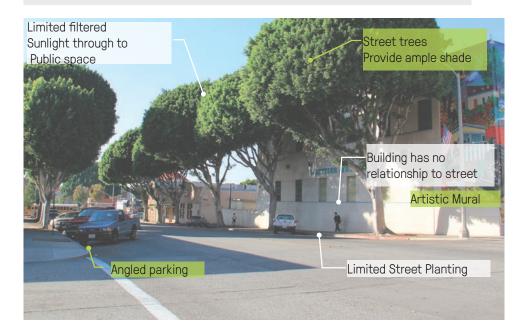
GREENLEAF AVE.



EXISTING SITE CONDITIONS

PROJECT SITE OBSERVATIONS

PHILADELPHIA ST.



BRIGHT AVE.



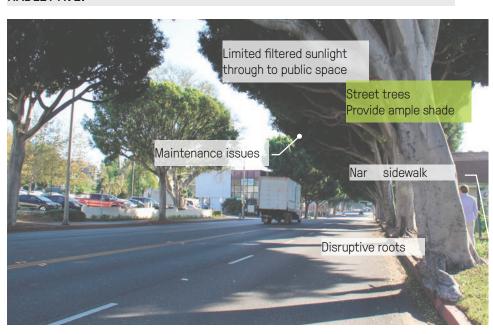
HADLEY AVE.



COMSTOCK AVE.



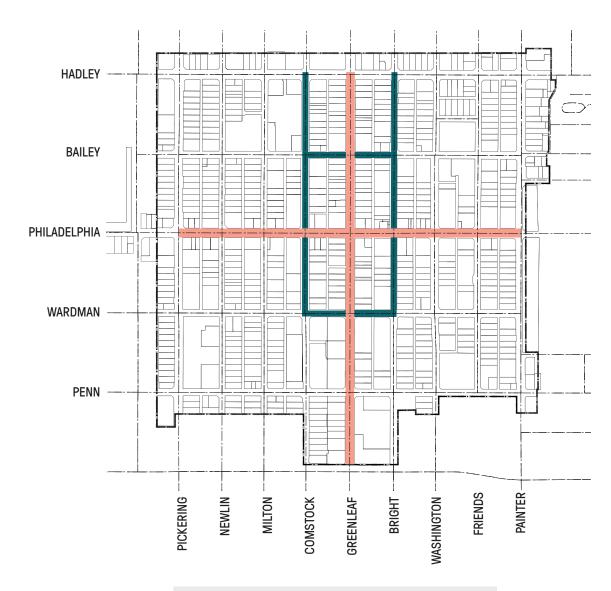
HADLEY AVE.



FRIENDS AVE.



STREET NETWORK | PASEOS + MID-BLOCK



STREET NETWORK

Uptown Whittier is composed of 340'x650' blocks running directly N-S lengthwise. The primary intersection is at Greenleaf and Philadelphia at the heart of Uptown Whittier.

LEGEND

Primary streetscape
Secondary streetscape



PASEOS + MID-BLOCK

Three Mid-Block connections exist along Greenleaf where Paseos break the building frontage and provide access to rear parking courts. These intimate spaces are great for gathering. A fourth Mid-Block connection could be created between Wardman and Penn.

LEGEND

Paseos

Mid-Blocks

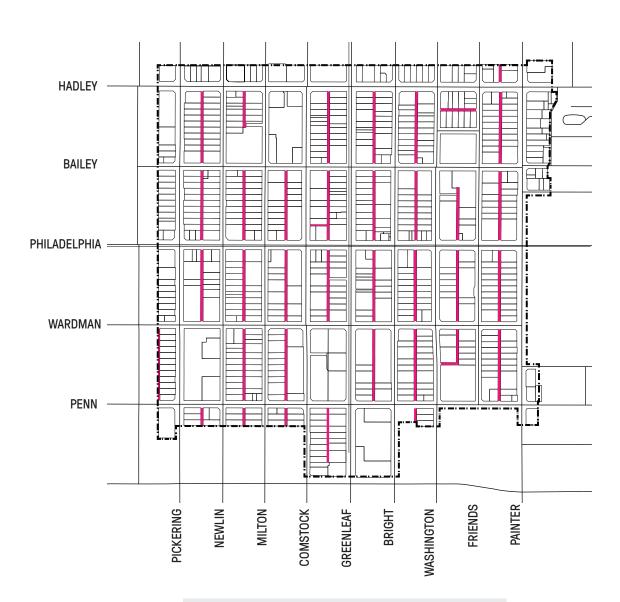




PARKS + PARKWAYS

Uptown Whittier only has one community park, Central Park, in the northeast corner of Uptown. Additional green spaces and plazas should be encouraged as available.

LEGEND Parks Parkways



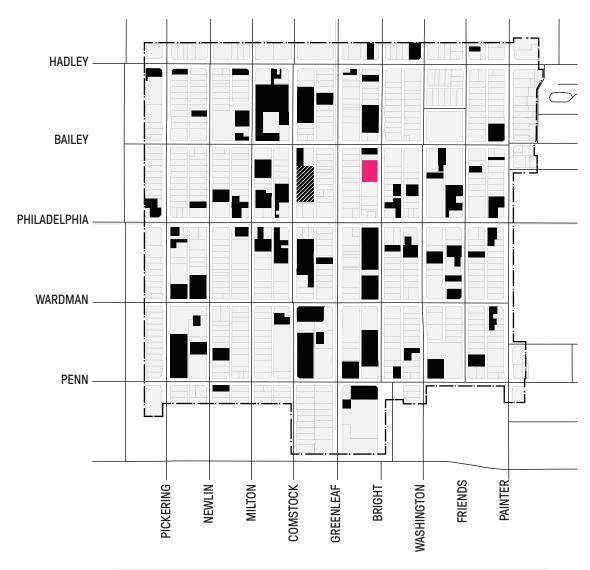
ALLEYWAYS

Alleyways split most blocks in half lengthwise, providing access to service areas and parking. Alleys are critical to making Uptown function but may also be potential spaces for temporary programming.

LEGEND

Alleyway

PARKING LOTS



UPTOWN SITEWIDE PARKING LOT CONDITIONS

Uptown Whittier has many parking lots and one public parking garage with another public garage planned. Additional parking garages will alleviate on-street parking demand and provide capacity for additional development in Uptown.

1,502

on-street parking spaces

2,281 off-street parking spaces

791

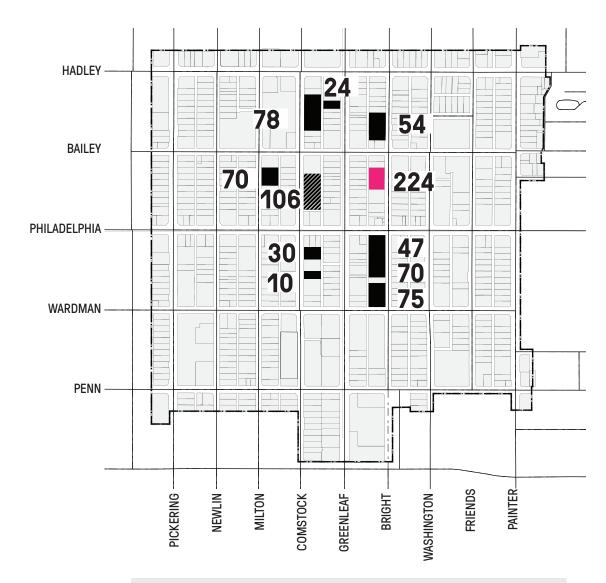
public spaces

LEGEND

Existing Surface Parking Lots

Existing Parking Garage

Proposed Parking Garage on Existing Lot



UPTOWN CORE AREA PARKING LOT CONDITIONS

The Core Area of Uptown Whittier is served by parking lots and a garage one-half block away, connected conveniently by paseos. This allows continuous frontage along Greenleaf, reinforcing a strong pedestrian character in the core.

788 existing parking lot spaces

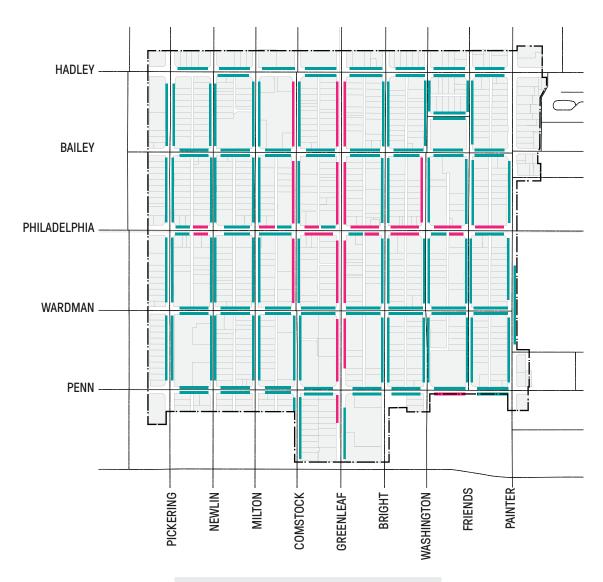
LEGEND

Existing Surface Parking Lots

Existing Parking Garage

Proposed Parking Garage on Existing Lot





STREET PARKING

Uptown Whittier has a mix of angled and parallel on-street parking, angled parking being concentrated primarily in areas that have more commercial businesses.

LEGEND

Angled On-Street Parking

Parallel On-Street Parking



STREET PARKING OCCUPANCY - WEEKEND - SATURDAY 7:00 PM

On-street parking has good availability on weekdays but gets relatively full on weekends.

weekday
61%
occupancy for on-street parking

85% occupancy for on-street parking

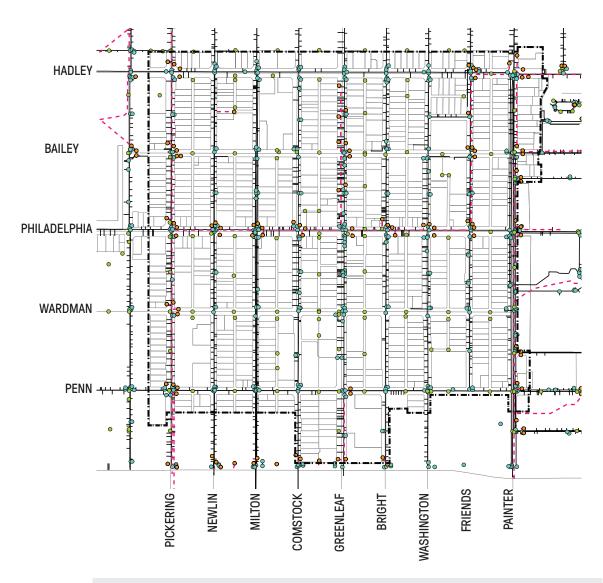
LEGEND

>85% Occupancy

70-85% Occupancy

<70% Occupancy</p>

INFRASTRUCTURE

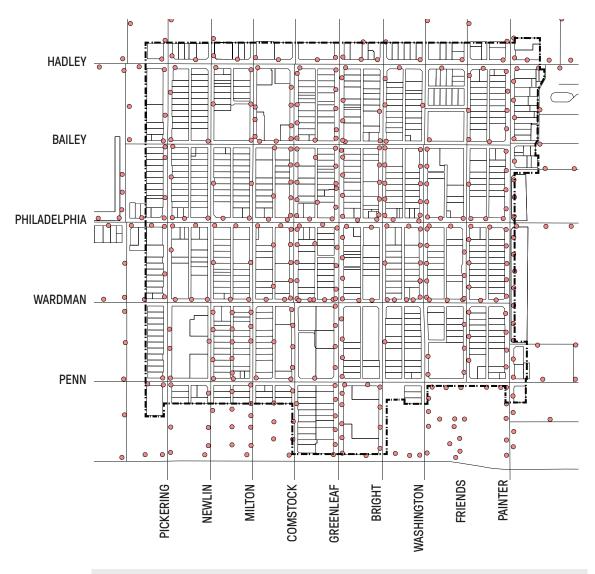


WATER NETWORK

Uptown Whittier has an extensive potable water, storm drainage and sewer system. General locations of the elements that make up these systems are shown above. As improvements are made, utilities should be surveyed in more detail.

LEGEND





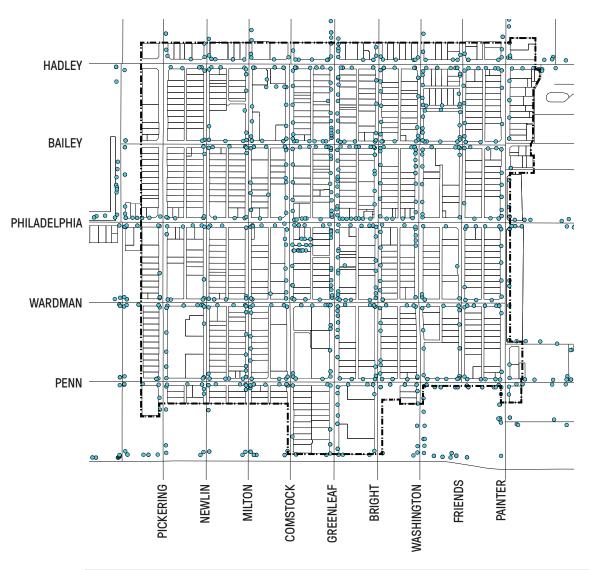
STREET LIGHTS

Uptown Whittier has four main types of street lights, a double decorative light, a single decorative light, a standard highway light on concrete pole and a standard highway light on wood pole.

LEGEND

Street Lights



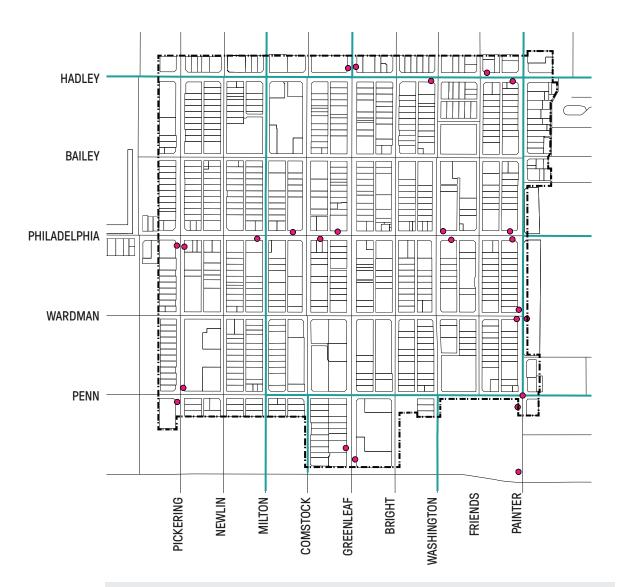


STREET SIGNS

Street signs within Uptown Whittier consist of primarily traffic signage but also include some wayfinding and navigational signage.

LEGEND

Street Signs



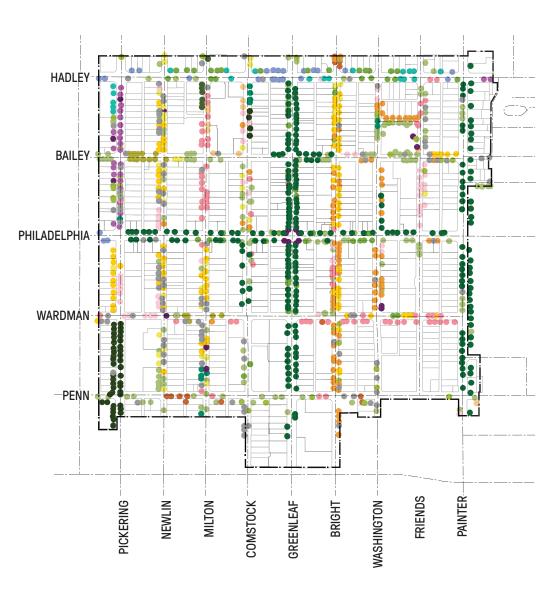
BUS STOPS + BIKE PATHWAYS

Uptwon Whittier contains a few N-S and E-W bike routes along Hadley, Penn, Milton and Painter. Additionally, bus lines running along Philadelphia, Hadley and Painter have several stops within Uptown.

LEGEND

Bike Route
Bus Stops

STREET TREES



1,317 trees **70** species

Uptown Whittier has over 1,300 trees composed of about 70 species. Of these, the predominant species include Indian Laurel Fig, Golden Trumpet Tree, Lemon Bottlebrush, Canary Island Pine, Sweetshade and Crape Myrtle.

Vacant Site

Silk Tree

Queen Palm

River She-Oak

Camphor TreeCarrotwoodPink Trumpet TreeCoast Live OakFern Pine

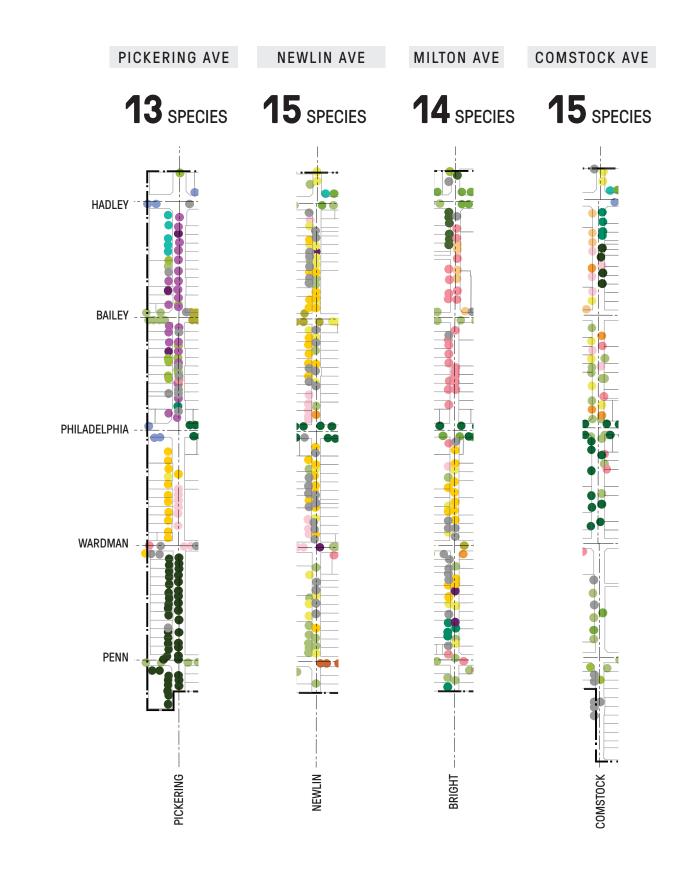
Chinese Pistache

Miscellaneous

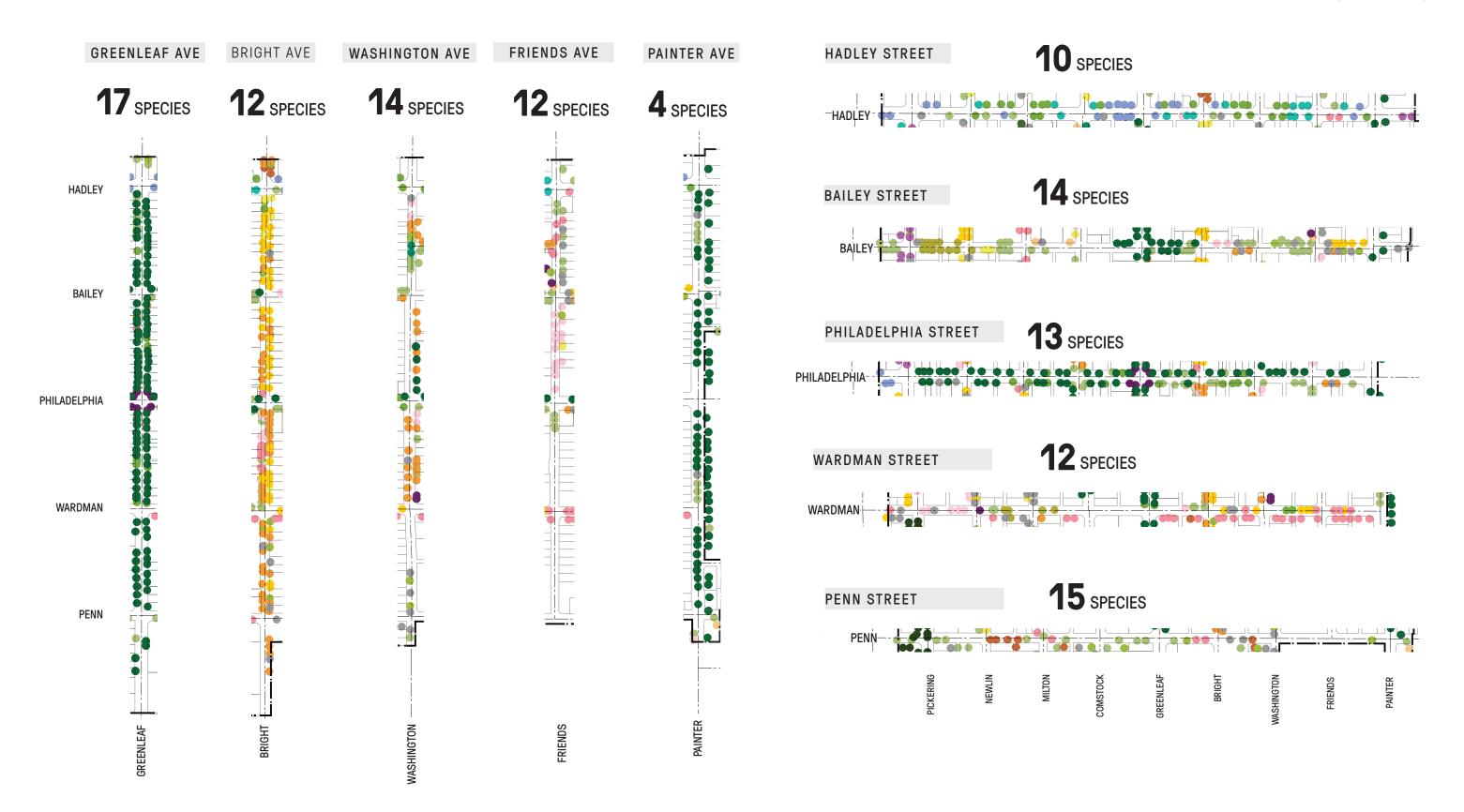
Indian Laurel FigCanary Island PineSweetshadeCrape Myrtle

Lemon Bottle BrushLong Leafed YellowwoodGolden Trumpet Tree

American SweetgumRound-Leafed Sweetgum



STREET TREES



EXISTING SITE CONDITIONS







OUALITIES OF STREET TREES

- Appearance
- Dappled Light
- Shade Canopy
- Urban Scale
- Cleanliness
- Low Maintenance
- Minimal Rooting
- Hardiness

ISSUES WITH WRONG STREET TREES

- No Dappled Light
- Messy
- Weak or Pest Prone
- Invasive Roots
- High Maintenance

BENEFITS OF STREET TREES

Street trees or community forests can be understood as dynamic systems of green infrastructure that provide cities with environmental, economic, and social benefits. Understanding the economic and cultural value of community forests and green infrastructure can enable municipalities to improve the quality of life and protect urban biodiversity for future generations.

Trees impart a distinctive character and identity to the city and to its neighborhoods. Whittier is noted for its strong tree canopy. To come home to a green and shaded community establishes a powerful sense of place.

Trees establish visual harmony and continuity along the city's streets. The experience of traveling through the community is immeasurably more pleasurable along treelined streets. If a single tree is a thing of beauty, a well designed street of trees can be a striking kinetic experience. Distinctive plantings on major streets also help with orientation, making the city more "imageable" and therefore easier to navigate.

Trees enrich the aesthetic experience of the city, adding pleasing shapes, colors, fragrance, texture, scale, and seasonal change.

Trees help diffuse noise. Dense foliage helps diffuse sound waves from traffic and other noises, and renders them less intrusive by visually screening their source.

Trees increase property value and act as a stimulus to economic development,

attracting new businesses and tourism. The beauty of a well-planted property and its surrounding street and neighborhood can raise residential property value by as much as 20 percent. Recent studies have revealed that people will travel farther, pay more for goods and services, and visit commercial districts more frequently—by an average of 12 percent—when trees and landscaping are present.

Trees enhance people's sense of connection to nature and history. Emotionally and symbolically, trees represent people's relation to that which is larger than themselves. They allow us to experience the natural world in a tangible form for which we feel responsible. Since trees, like people, grow and change through time, we identify with them. And since they often live longer than we do, they link us to times beyond our own, spanning past and future generations.

Trees enhance civic pride and involvement. Tree planting programs allow citizens to participate in creating a city they can be proud of. Trees provide shade and evaporative cooling, helping to combat the effects of urban heat islands, and reducing energy costs and consumption. In urban areas, shaded surfaces may be 20-45 degrees cooler and evapotranspiration can reduce peak summer temperatures by 2-9 degrees.

Trees moderate wind. When trees are planted strategically, they not only reduce wind exposure but can reduce heating bills in the winter by 2 to 8 percent.

EXISTING SITE CONDITIONS

EXISTING FICUS TREES

INDIAN LAUREL FIG (FICUS MICROCARPA)

HEIGHT / WIDTH:

40-60' Tall / 50-60' Wide

CHARACTERISTICS:

Non-native evergreen tree Oval or rounded shape

AVG SPACING:

28-36' on center

GROWTH RATE:

24 inches per season

PLANT NEEDS:

Medium water needs / full sun

to partial shade

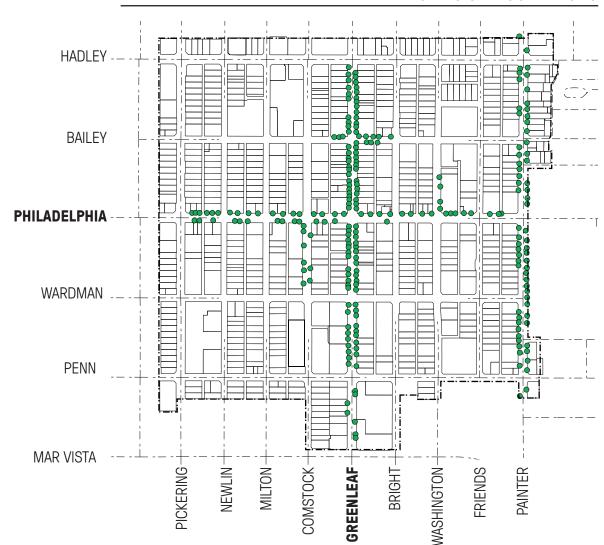
Indian Laural Figs (or Ficus Trees) are a promenant feature of Uptown Whittier, especially along Greenleaf, Philadelphia and Painter. While grand in stature, these trees have many problems that have resulted in the Uptown Whittier Specific Plan identifying them for removal and replacement with street trees more subtable to Uptown's urban condition.

241 FICUS TREES

CONCERNS:

Medium/Weak Branch Strength Root Damage Potential Messy Fruit Droppings Maintenance Issues Lack of Light Penetration Host to Pests

Identified for Removal in Uptown Whittier Specific Plan





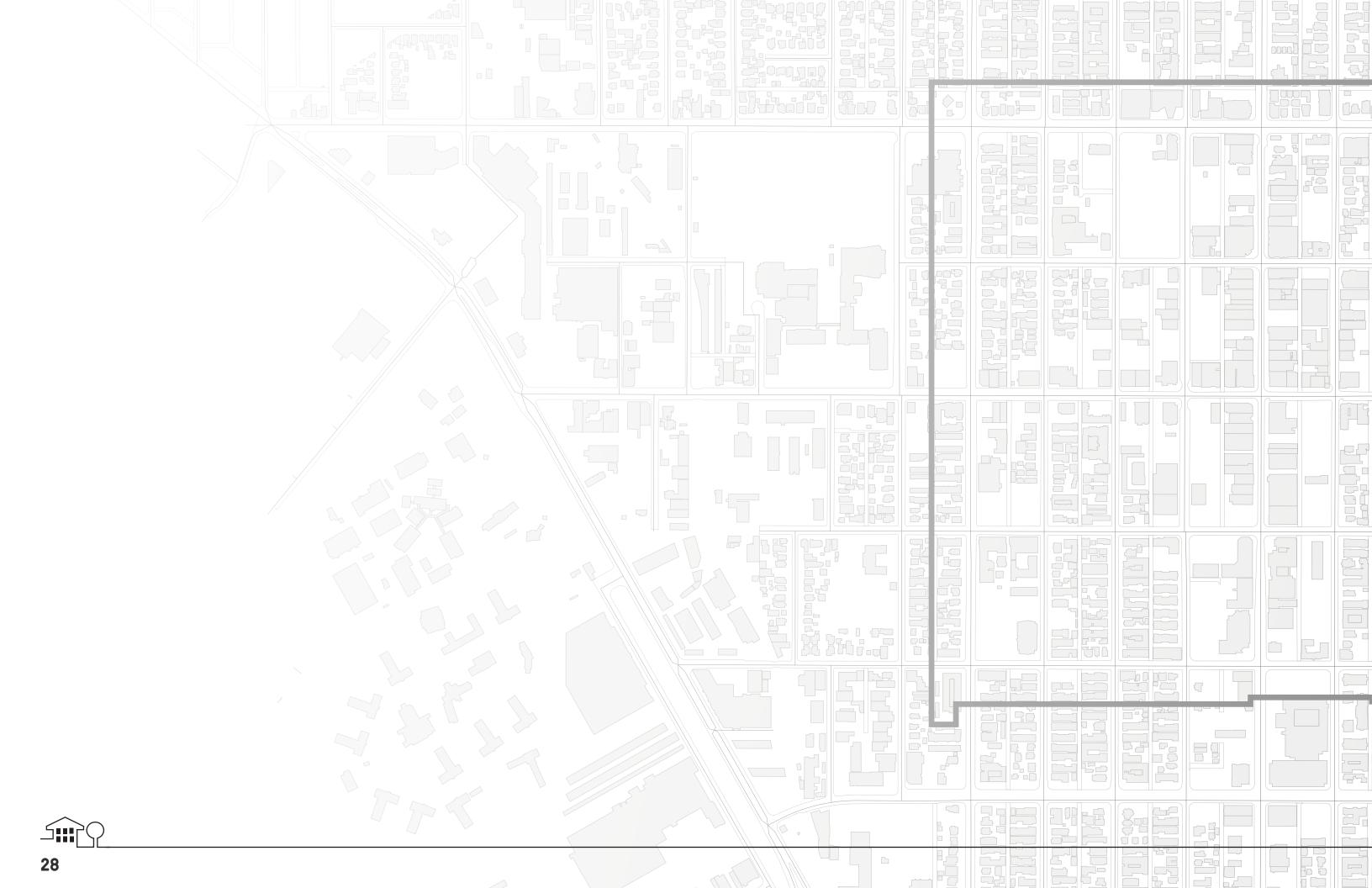
OVERGROWN TREE CANOPY

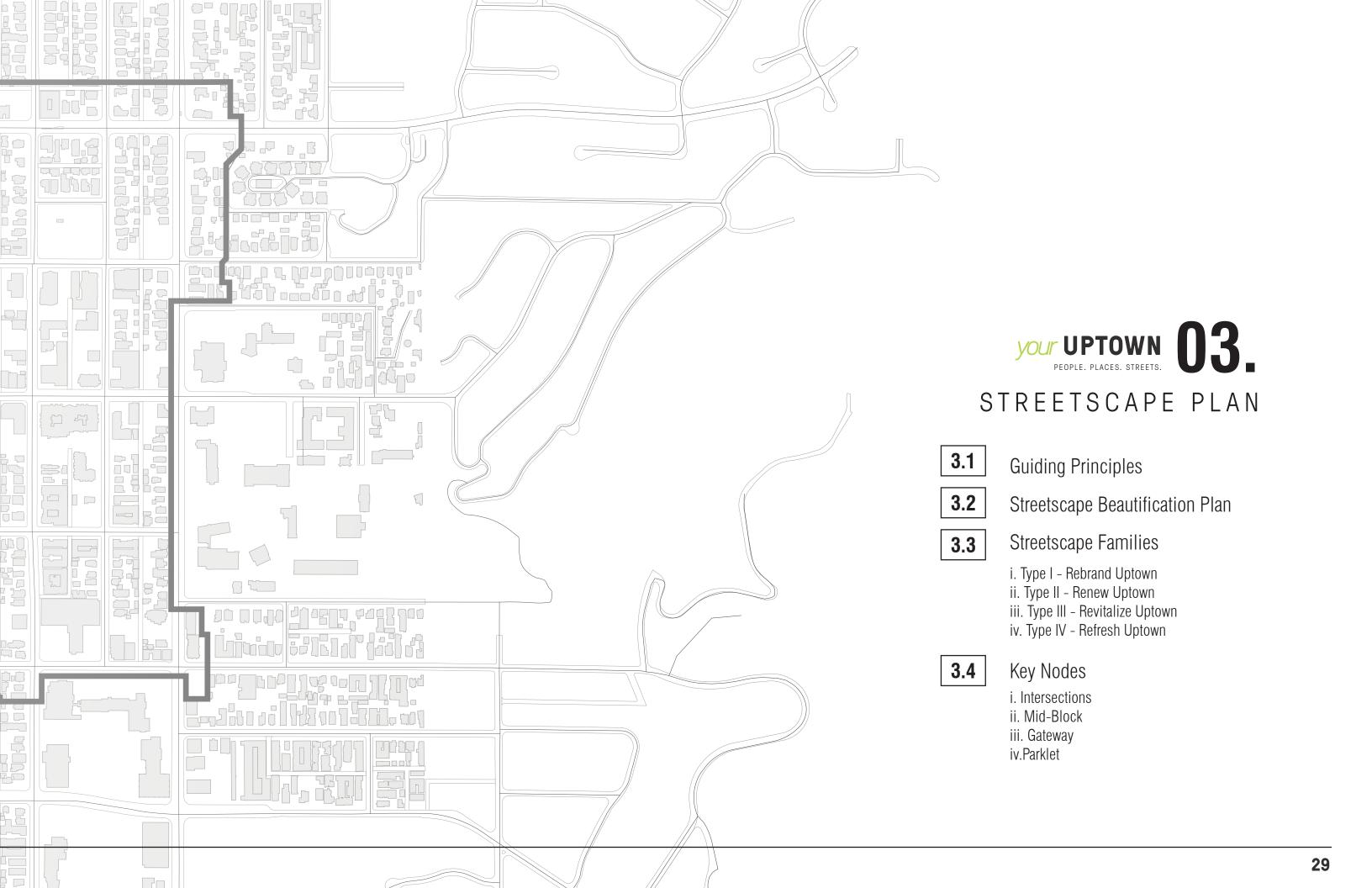


STAINED SIDEWALKS FROM FALLING TREE FRUIT



DISRUPTIVE TREE ROOTS CAUSE THE SIDEWALK TO HEAVE





DESIGN Guidelines

Good streetscape design begins with an approach that emphasizes wholeness, considering how various elements interact to create an overall streetscape composition. The design guidelines found in this section help establish a set of streetscape families that define an identity for each street within Uptown Whittier.











PEDESTRIAN ORIENTATION

- Human Scale
- Maintain A 5-Minute Walkability Index
- Create Memorable Places For People
- Define Street Edges + Corners
- Enliven The Streetscape



INTERCONNECTED STREET SYSTEM

- Multiple Access Points + Routes
- Easy Accessibility
- Integrated Mobility: Bike, Public Transport, Car
- Disperse Traffic



QUALITY OF PUBLIC REALM

- A Public Realm For Moving, Gathering, And Celebrating
- A Series Of Outdoor Rooms
- Urban Architecture And Landscape Design Sets The Foundation
 For The Physical Definition Of Streets + Public Spaces



DISTINCT CHARACTER

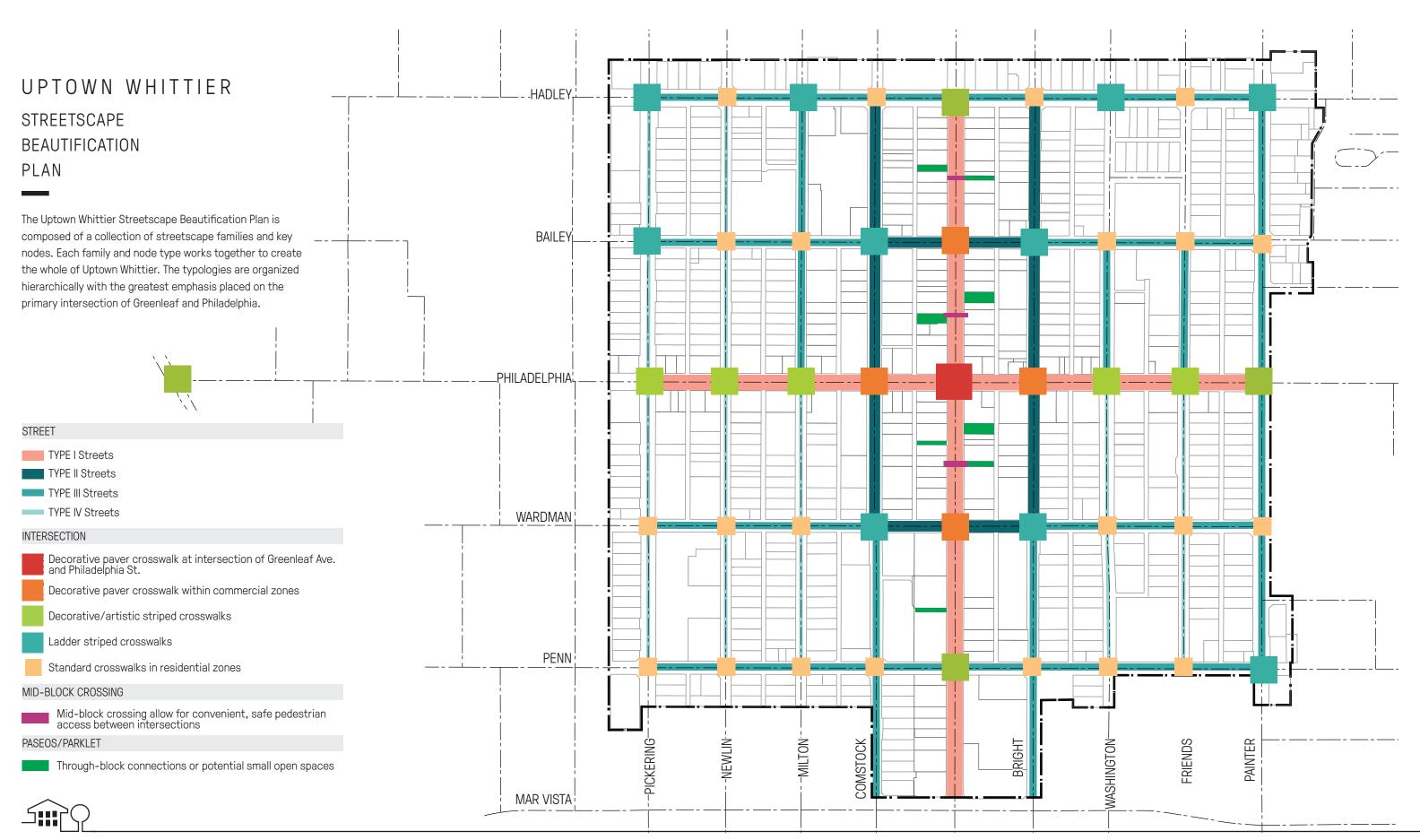
- Preserve And Renew Historic Roots
- New Development That Enriches The Quality Of Existing Urban Places
- Character That Is Responsive And Complementary



SMART TRANSPORTATION + PARKING

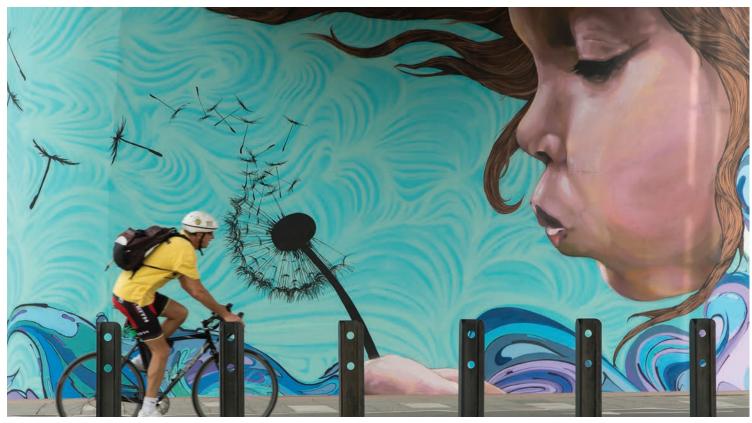
- Streets For All
- Transportation Choices That Are Affordable And Environmentally Sensitive
- Park Once Strategy

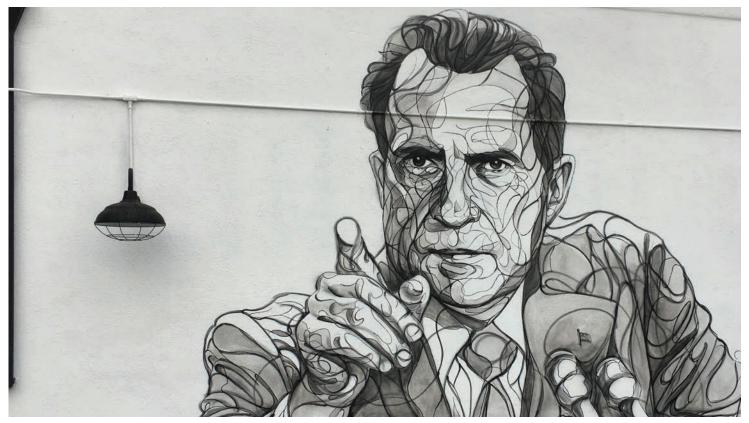
These five principles have been used to guide the development of design concepts for the street families of Uptown Whittier. These principles are general and have been edited in relation to the specific conditions of each streetscape.











The Streetscape Beautification Plan

STREETSCAPE **FAMILIES**

The Streetscape Families are arranged in a hierarchical order of streetscape improvement standards: Type I, Type II, Type III and Type IV Standards. Each family type provides guidelines for street trees, paving, understory planting and street furniture to define the overall character of the street.

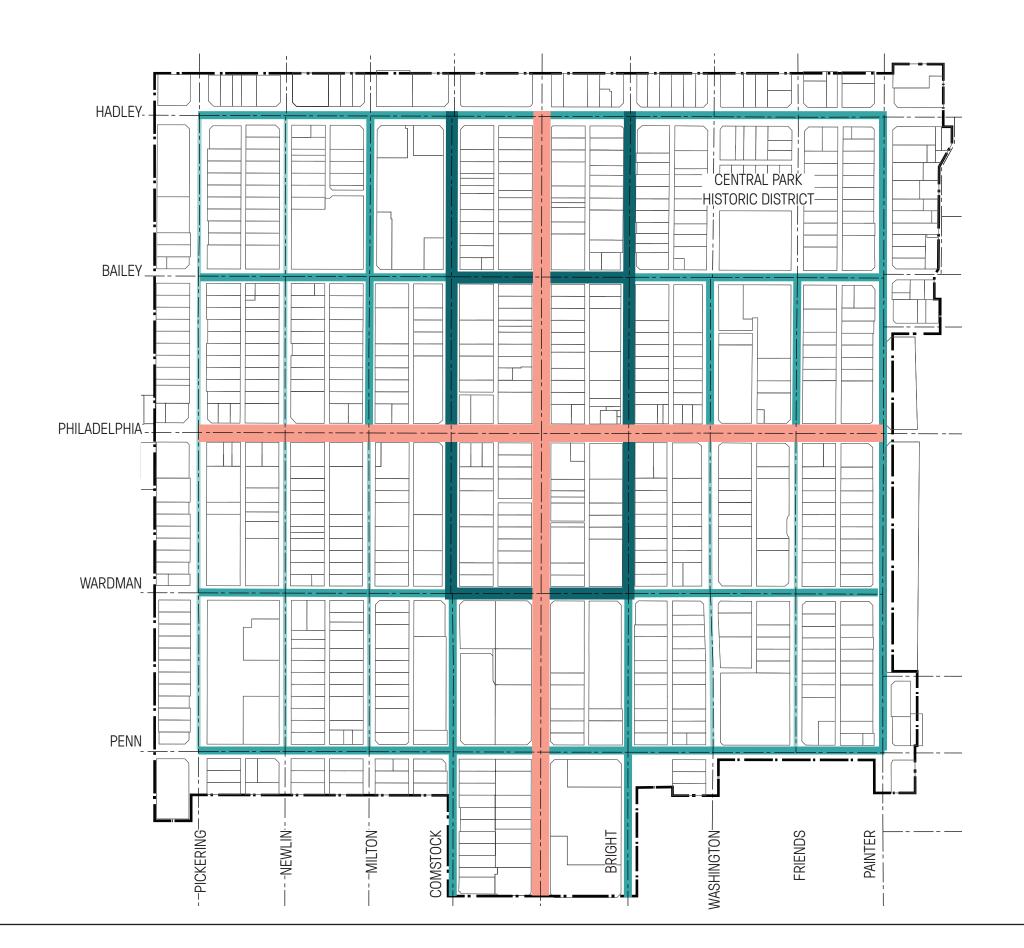
Street Families are further defined by ROW width. Section markers identify the general stretch of street where a Streetscape Family applies however as ROW widths vary, alternate Streetscape Families within the same Type may be applied.











REBRAND UPTOWN

Type I Streets define the primary axes of Uptown

RENEW UPTOWN

Type II Streets define the core commercial area of Uptown

REVITALIZE UPTOWN

Type III Streets define the secondary commercial area of Uptown

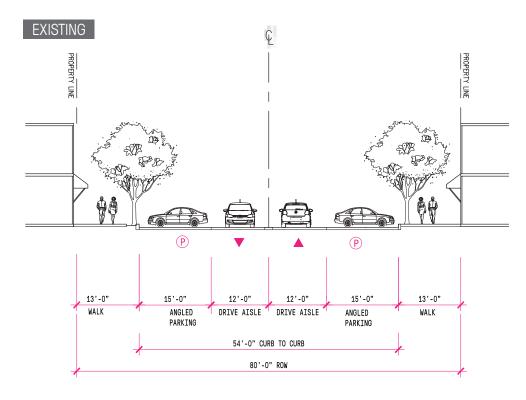
REFRESH UPTOWN

Type IV Standard Streets are primarily Residential Streets

STREETSCAPE FAMILIES

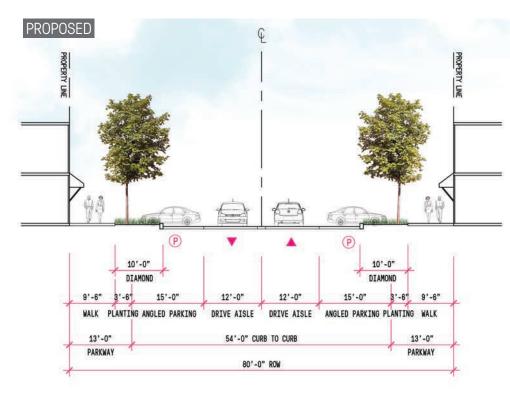
REBRAND UPTOWN

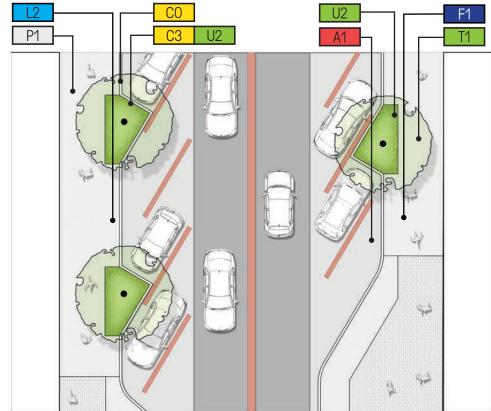
TYPE I STANDARD 80' ROW | ANGLED PARKING

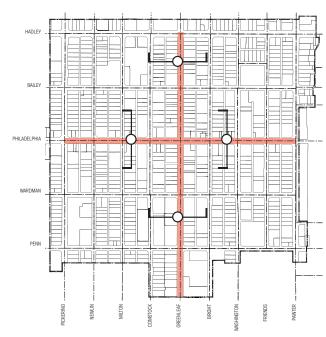


KEY DESIGN CONCEPT

Greenleaf and Philadelphia are the primary commercial streets of Uptown Whittier and increasing space for both pedestrian use and better tree pit sizes are a primary goal for the redesign of both of these streets. Within areas of angled parking, tree pits are moved into the angled parking spaces to provide more sidewalk space and room for tree pits.







*Where angled parking is to remain.

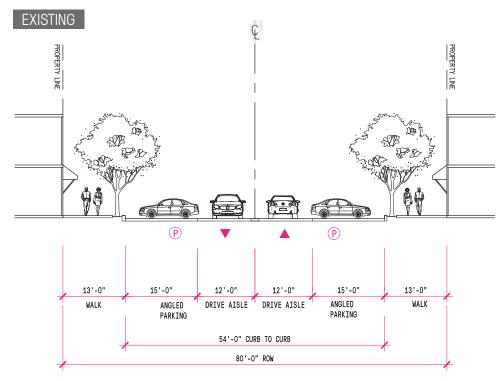
| PROPOSED STREET STANDARDS | |
|---------------------------|-------------------------------------|
| VEHICULAR | |
| Right of Way | 80' |
| Sidewalk Width | Varies (10' To 13' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Angled |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Diamond Planter |
| Tree Spacing | Varies (Existing), 30' Maximum Typ. |
| Tree Type | Existing to be phased to Canopy |
| Sustainable Strategies | Enhanced Bioswale Tree Pits |

| DESIGN PALETTE | | |
|----------------|-------------------------------|--|
| | ITEM | |
| P1 | City Standard Concrete Paving | |
| CO | Existing Curb | |
| C3 | Diamond Planter Curb | |
| T1 | Canopy Tree | |
| U2 | Streetscape Planting | |
| F1 | Standard Seating | |
| L2 | Single Decorative Light | |
| A1 | Accessible Parking Spot | |



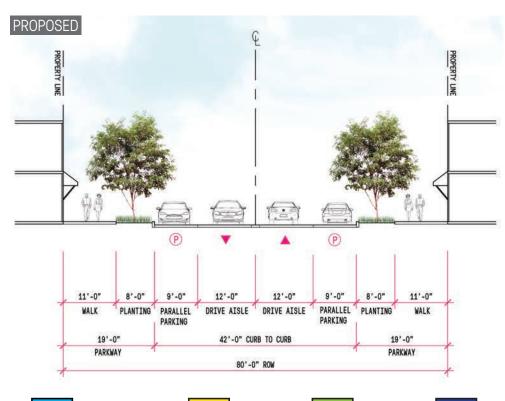
REBRAND UPTOWN

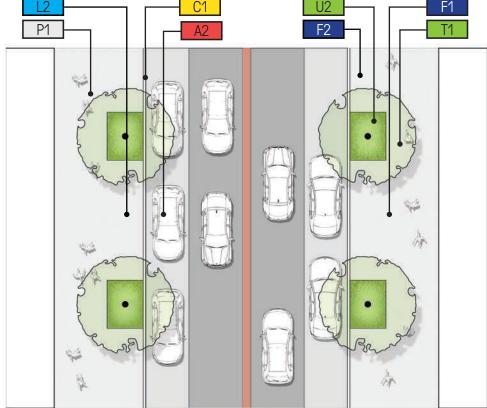
TYPE | STANDARD 80' ROW | PARALLEL PARKING

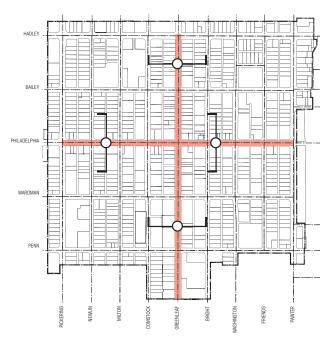


KEY DESIGN CONCEPT

Greenleaf and Philadelphia are the primary commercial streets of Uptown Whittier and increasing space for both pedestrian use and better tree pit sizes are a primary goal for the redesign of both of these streets. Near intersections, angled parking is replaced with parallel parking, providing much wider sidewalks and properly sized tree pits as well as areas for valet, rideshare and delivery loading.







*Where parallel parking is to remain or is added.

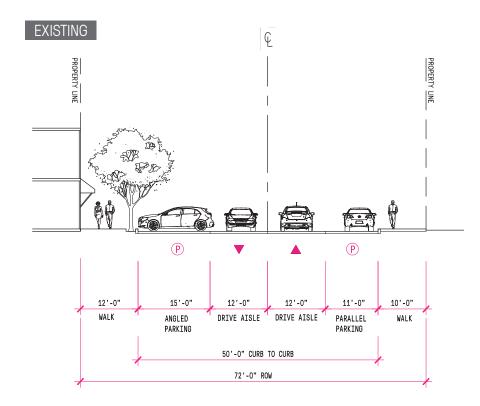
| PROPOSED STREET STANDARDS | |
|---------------------------|------------------------------------|
| VEHICULAR | |
| Right of Way | 80' |
| Sidewalk Width | Varies (11' To 19' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Parallel |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Sidewalk Planter |
| Tree Spacing | 30' Maximum Typ. |
| Tree Type | Canopy |
| Sustainable Strategies | n/a |

| DESIGN | PALETTE |
|--------|-------------------------------|
| | ITEM |
| P1 | City Standard Concrete Paving |
| C1 | New Curb |
| T1 | Canopy Tree |
| U2 | Streetscape Planting |
| F1 | Standard Seating |
| F2 | Group Seating |
| L2 | Single Decorative Light |
| A2 | Loading Zone |

The Streetscape Beautification Plan

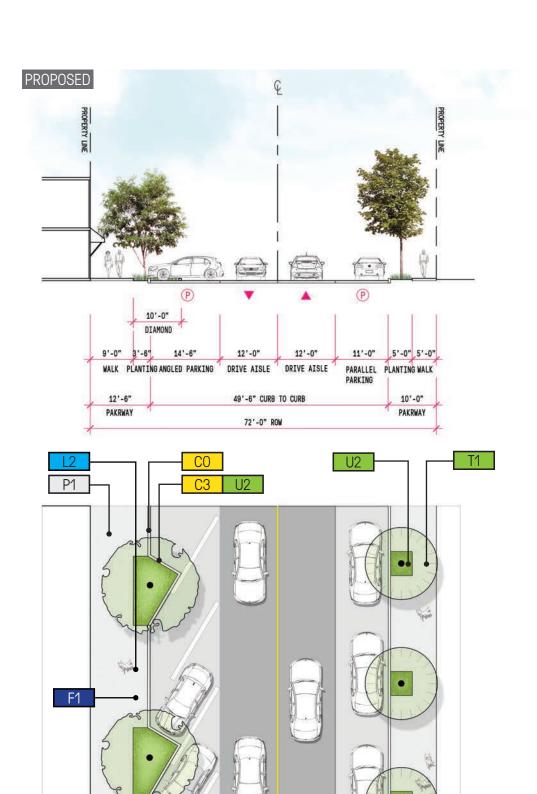
RENEW UPTOWN

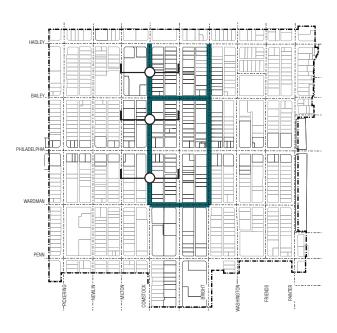
TYPE II STANDARD 68'-72' ROW | ANGLED PARKING ONE SIDE



KEY DESIGN CONCEPT

Comstock is a commercially oriented street with a unique street condition of angled parking on one side and parallel parking on the other. Larger tree planting areas are proposed by providing modified diamonds that encroach into the existing angled parking areas.





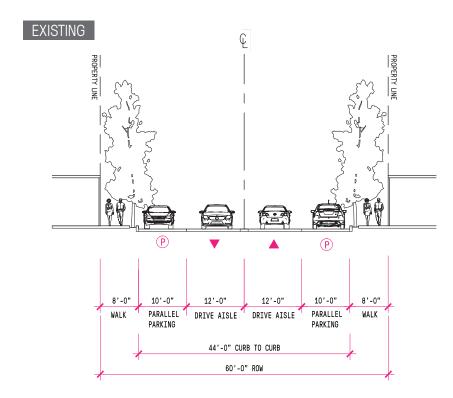
| <u> </u> | |
|---------------------------|-------------------------------------|
| PROPOSED STREET STANDARDS | |
| VEHICULAR | |
| Right of Way | 68'-72' |
| Sidewalk Width | Varies (5' to 10' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Varies per Side (Angled/Parallel) |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Varies (Diamond Planter, Tree Pit) |
| Tree Spacing | Varies (Existing, 30' Maximum Typ.) |
| Tree Type | Canopy |
| Sustainable Strategies | Enhanced Bioswale Tree Pits |

| DESIGN | PALETTE |
|--------|-------------------------------|
| | ITEM |
| P1 | City Standard Concrete Paving |
| CO | Existing Curb |
| C3 | Diamond Planter Curb |
| T1 | Canopy Tree |
| U2 | Streetscape Planting |
| F1 | Standard Seating |
| L2 | Single Decorative Light |
| A1 | Accessible Parking Spot |



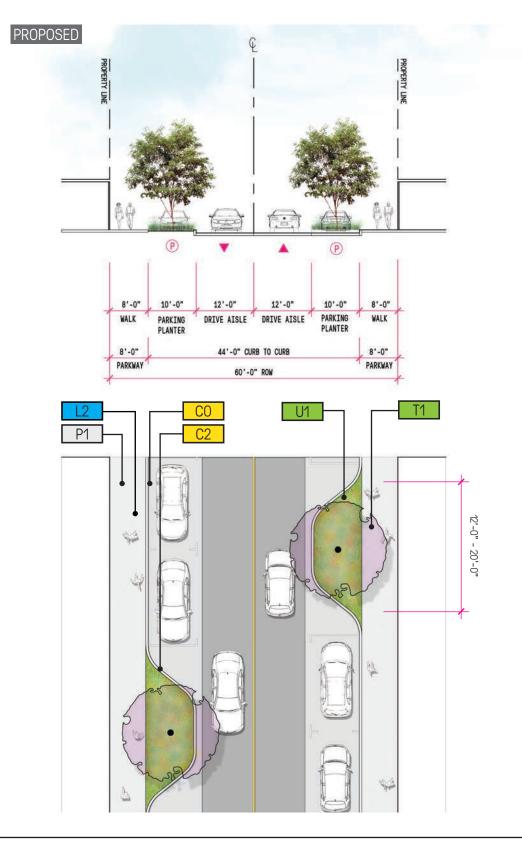
RENEW UPTOWN

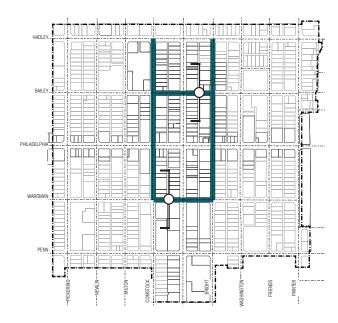
TYPE II STANDARD 60' ROW | PARALLEL PARKING



KEY DESIGN CONCEPT

East-West streets within Uptown Whitter generally have a 60' ROW with a 44' pavement width which limits the room for proper tree pit sizes. To increase planting areas and provide better sidewalk space, parking planters are proposed on these commercially oriented blocks. At 60' on-center, offset from one side of the street to the other, these parking planters may remove some of the existing parking space as necessary.





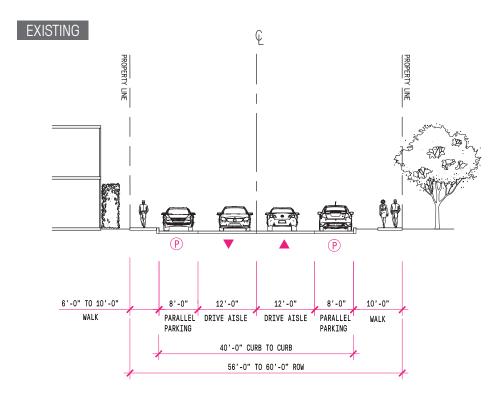
| PROPOSED STREET STANDARDS VEHICULAR Right of Way 60' Sidewalk Width 8' Clear Traffic Lanes 1 Lane Each Way (12' Travel Lanes) | | |
|---|---------------------------|--|
| Right of Way 60' Sidewalk Width 8' Clear | PROPOSED STREET STANDARDS | |
| Sidewalk Width 8' Clear | | |
| | | |
| Traffic Lanes 1 Lane Each Way (12' Travel Lanes) | | |
| | | |
| Parking Lanes Parallel | | |
| Bike Shared With Vehicular Traffic | | |
| LANDSCAPE | | |
| Tree Location Parking Planter | | |
| Tree Spacing 60' Maximum Typ., Offset | | |
| Tree Type Canopy | | |
| Sustainable Strategies Enhanced Bioswale Tree Pits | | |

| DESIGN | DESIGN PALETTE | |
|--------|-------------------------------|--|
| | ITEM | |
| P1 | City Standard Concrete Paving | |
| C1 | Existing Curb | |
| C2 | Garden Planter Curb | |
| T1 | Canopy Tree | |
| U1 | Garden Planting | |
| L2 | Single Decorative Light | |

The Streetscape Beautification Plan

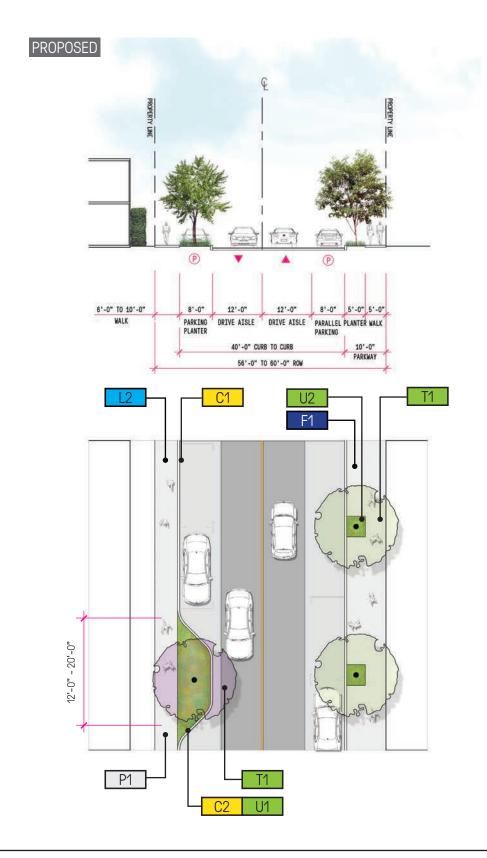
RENEW UPTOWN

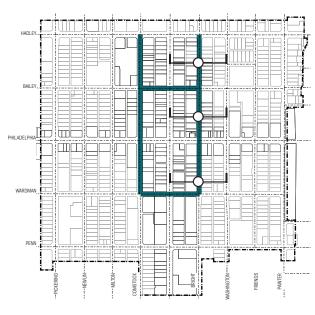
TYPE II STANDARD 55'-60' ROW | PARALLEL PARKING



KEY DESIGN CONCEPT

Bright has a ROW that varies from about 56'-60'. This ROW is very limiting to get proper tree pit sizes and enough space for pedestrian oriented sidewalks. Parking planters are proposed to provide adequate areas for tree pits and improve the sidewalk widths. At 60' on-center, offset from one side of the street to the other, these parking planters may remove some of the existing parking space as necessary.





| PROPOSED STREET STANDARDS | |
|---------------------------|---|
| VEHICULAR | |
| Right of Way | 55'-60' |
| Sidewalk Width | Varies (5' To 10' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Parallel |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Sidewalk Tree Pit and Optional Parking Planter |
| Tree Spacing | Varies (60' Maximum Typ. for Parking Planter, 30' Maximum Typ. for Sidewalk Tree Pit) |
| Tree Type | Canopy, Accent Canopy |
| Sustainable Strategies | Enhanced Bioswale Tree Pits |

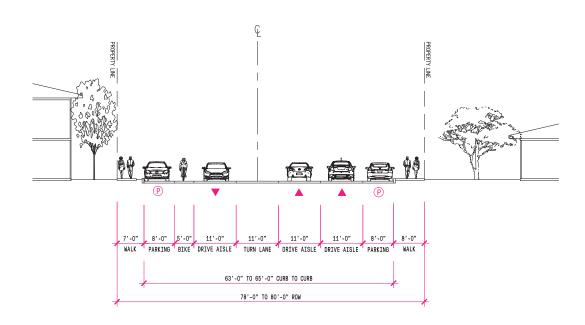
| DESIGN | PALETTE |
|--------|-------------------------------|
| | ITEM |
| P1 | City Standard Concrete Paving |
| C1 | New Curb |
| C2 | Garden Planter Curb |
| T1 | Canopy Tree |
| T2 | Accent Tree |
| U1 | Garden Planting |
| U2 | Streetscape Planting |
| F1 | Standard Seating |
| L2 | Single Decorative Light |



REVITALIZE UPTOWN

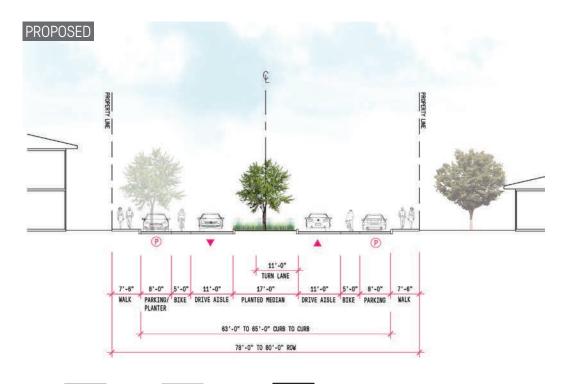
TYPE III STANDARD 78'-80' ROW | TURN LANE

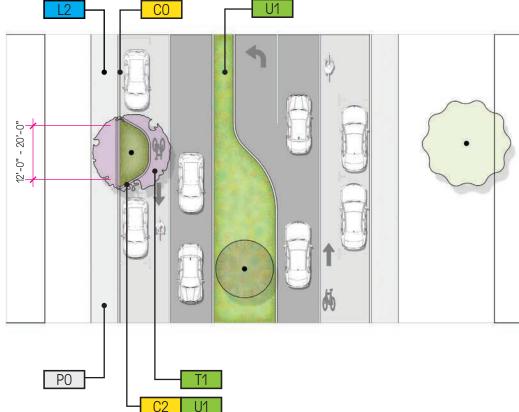


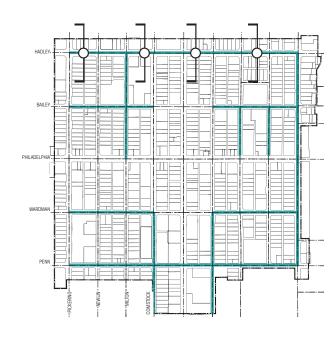


KEY DESIGN CONCEPT

Hadley is a major East-West connector with an existing bike lane and travel lane east-bound and two travel lanes west-bound in addition to a center turn lane. To the East of the project site, Hadley has just one travel lane in each direction and bike lanes in each direction as well as intermittent planted medians within the area devoted to turn lanes. It is proposed to convert the portion of Hadley within the project area to a similar section, replacing one of the west-bound travel lanes with a bike lane and adding planted medians where possible. In addition Hadley does not currently have enough parkway width to support street trees, so parking planters will be provided to allow street tree planting. At 60' on-center, off set from one side of the street to the other, these parking planters may remove some of the existing parking space as necessary.







| PROPOSED STREET STANDARDS | |
|---------------------------|------------------------------------|
| VEHICULAR | |
| Right of Way | 78'-80' |
| Sidewalk Width | Varies (7'-8" Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Parallel |
| Bike | 1 Lane Each Way (5' Travel Lanes) |
| LANDSCAPE | |
| Tree Location | Optional Parking Planter, Median |
| Tree Spacing | 60' Maximum Typ. |
| Tree Type | Accent Canopy, Accent Median |
| Sustainable Strategies | Enhanced Bioswale Tree Pits |

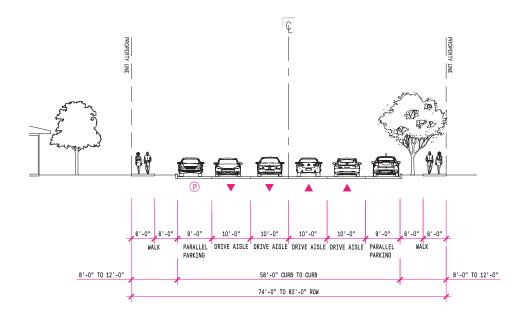
| DESIGN | PALETTE |
|--------|--|
| | ITEM |
| PO | Existing City Standard Concrete Paving |
| CO | Existing Curb |
| C2 | Garden Planter Curb |
| T1 | Canopy Tree |
| U1 | Garden Planting |
| L2 | Single Decorative Light |

The Streetscape Beautification Plan

REVITALIZE UPTOWN

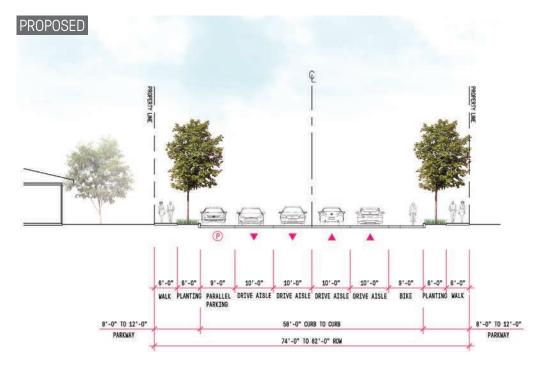
TYPE III STANDARD 74'-82' ROW

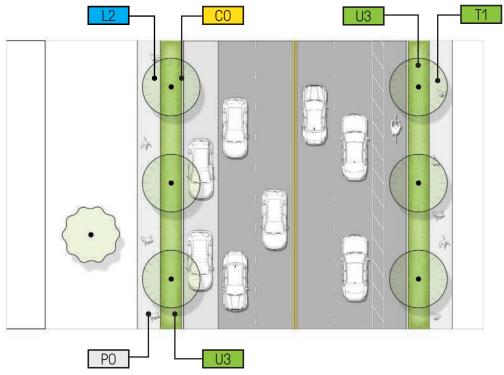
EXISTING

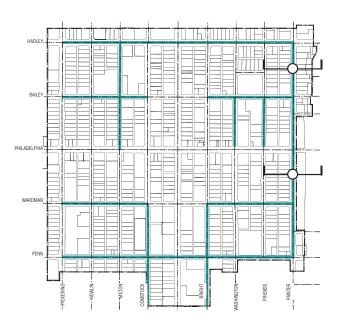


KEY DESIGN CONCEPT

Painter is a major North-South connector with adequate width for both sidewalks and a planting strip. Continuous 6' wide sidewalks adjacent to a 6' wide plating strip is proposed.







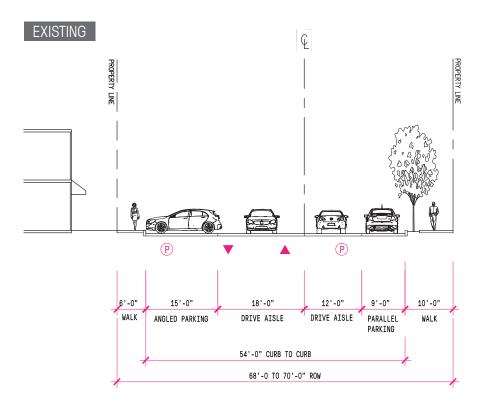
| PROPOSED STREET STANDARDS | |
|---------------------------|--|
| VEHICULAR | |
| Right of Way | 74'-82' |
| Sidewalk Width | Varies (6' Clear) |
| Traffic Lanes | 2 Lane Each Way (10' Travel Lanes) |
| Parking Lanes | Parallel One Side (Southbound) |
| Bike | 1 Lane (Northbound, 5' Bike Lane with 3' Buffer) |
| LANDSCAPE | |
| Tree Location | Parkway |
| Tree Spacing | 30' Maximum Typ. |
| Tree Type | Canopy |
| Sustainable Strategies | n/a |

| DESIGN PALETTE | |
|----------------|--|
| | ITEM |
| P0 | Existing City Standard Concrete Paving |
| CO | Existing Curb |
| T1 | Canopy Tree |
| U3 | Standard Planting |
| L2 | Single Decorative Light |



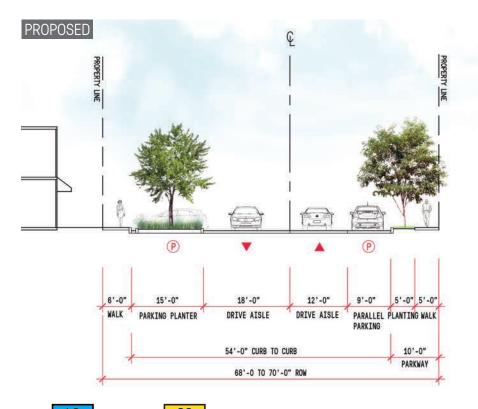
REVITALIZE UPTOWN

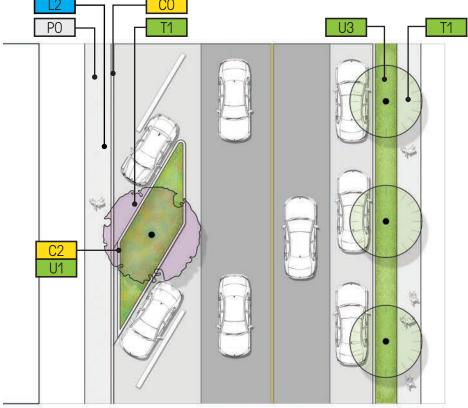
TYPE III STANDARD 68'-70' ROW | ANGLED PARKING ONE SIDE

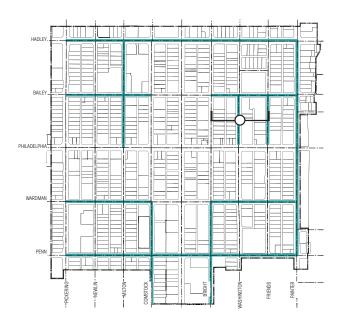


KEY DESIGN CONCEPT

Washington is a mixed commercial and residential street with angled parking on one side and parallel parking on the other. Parking planters are proposed adjacent to existing sidewalks on the angled parking side.







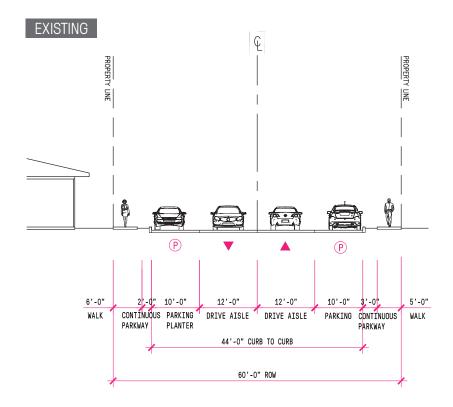
| PROPOSED STREET STANDARDS | |
|---------------------------|---|
| VEHICULAR | |
| Right of Way | 68'-70' |
| Sidewalk Width | Varies (5' To 6' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Northbound Lane, 18' Southbound Lane) |
| Parking Lanes | Varies (Angled/Parallel) |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Parkway and Optional Parking Planter |
| Tree Spacing | Varies (60' Maximum Typ. for Parking Planter, 30' Maximum Typ. for Parkway) |
| Tree Type | Canopy |
| Sustainable Strategies | Enhanced Bioswale Tree Pits |

| DESIGN | PALETTE |
|--------|--|
| | ITEM |
| PO | Existing City Standard Concrete Paving |
| CO | Existing Curb |
| C2 | Garden Planter Curb |
| T1 | Canopy Tree |
| U1 | Garden Planting |
| U3 | Parkway Planting |
| L2 | Single Decorative Light |

The Streetscape Beautification Plan

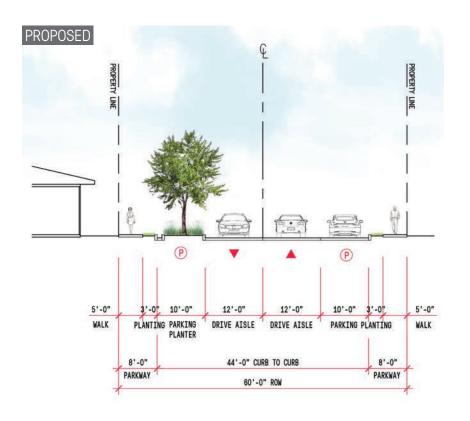
REVITALIZE UPTOWN

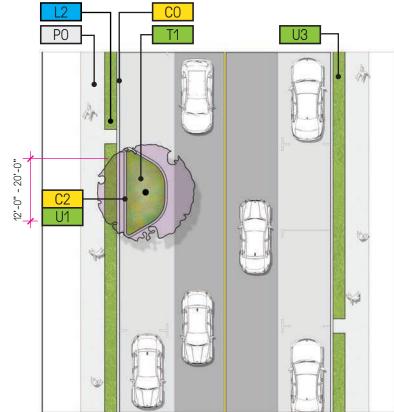
TYPE III STANDARD **60' ROW**

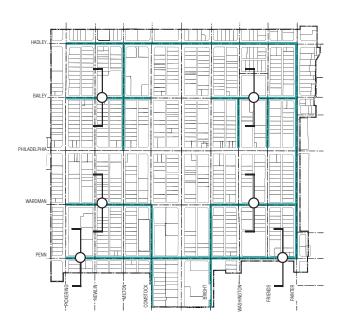


KEY DESIGN CONCEPT

East-West streets within Uptown Whitter generally have a 60' ROW with a 44' pavement width which limits the room for proper tree pit sizes. To increase planting areas and provide better sidewalk space, parking planters are proposed on these mixed commercial and residentially oriented blocks adjacent to the existing sidewalk. At 60' on-center, offset from one side of the street to the other, these parking planters may remove some of the existing parking space as necessary.







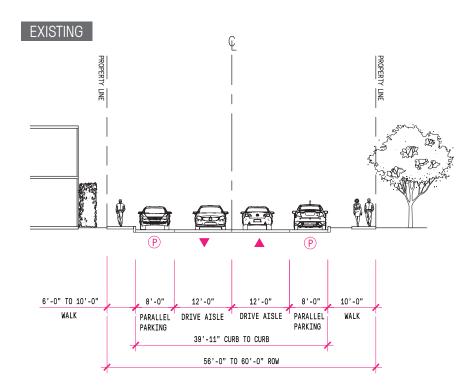
| PROPOSED STREET STANDARDS | | |
|---------------------------|------------------------------------|--|
| VEHICULAR | | |
| Right of Way | 60' | |
| Sidewalk Width | Varies (5' Clear) | |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) | |
| Parking Lanes | Parallel | |
| Bike | Shared With Vehicular Traffic | |
| LANDSCAPE | | |
| Tree Location | Optional Parking Planter | |
| Tree Spacing | 60' Maximum Typ. | |
| Tree Type | Accent Canopy | |
| Sustainable Strategies | Enhanced Bioswale Tree Pits | |
| | | |

| DESIGN | PALETTE |
|--------|-------------------------------|
| | ITEM |
| PO | City Standard Concrete Paving |
| CO | Existing Curb |
| C2 | Garden Planter Curb |
| T1 | Canopy Tree |
| U1 | Garden Planting |
| U3 | Standard Planting |
| L2 | Single Decorative Light |



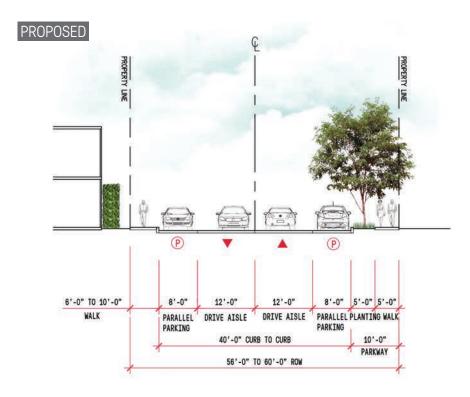
REVITALIZE UPTOWN

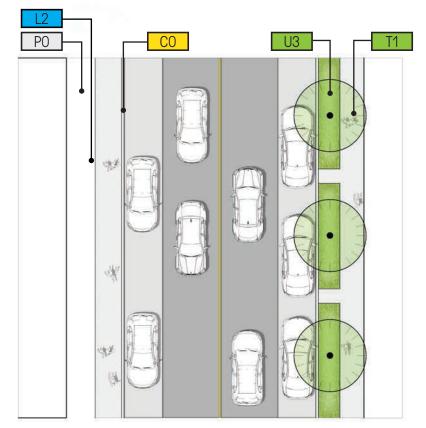
TYPE III STANDARD **56-60' ROW**

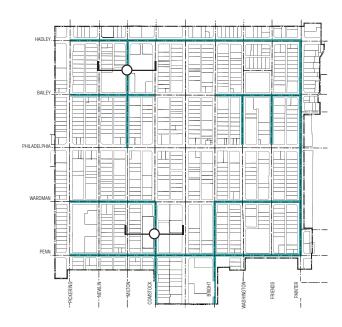


KEY DESIGN CONCEPT

Several north-south streets have a ROW that varies from about 56'-60'. This ROW is very limiting to get proper tree pit sizes and enough space for pedestrian oriented sidewalks. Parking planters are proposed adjacent to existing parkways that are less than 10' wide while continuous 5' planting strips should be installed on 10' parkways to provide a minimum 5' wide sidewalk and the best possible tree pit condition. At 60' on-center, offset from one side of the street to the other, these parking planters may remove some of the existing parking space as necessary.







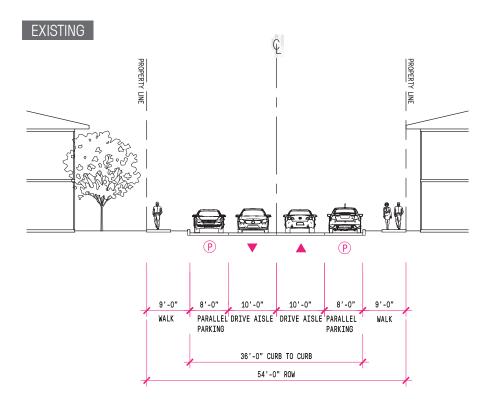
| PROPOSED STREET STANDARDS | |
|---------------------------|------------------------------------|
| VEHICULAR | |
| Right of Way | 56'-60' |
| Sidewalk Width | Varies (5' To 10' Clear) |
| Traffic Lanes | 1 Lane Each Way (12' Travel Lanes) |
| Parking Lanes | Parallel |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Parkway One Side, Optional Parking |
| | Planter |
| Tree Spacing | 30' Maximum Typ. |
| Tree Type | Canopy |
| Sustainable Strategies | n/a |

| DESIGN | PALETTE |
|--------|--|
| | ITEM |
| PO | Existing City Standard Concrete Paving |
| CO | Existing Curb |
| T1 | Canopy Tree |
| U3 | Parkway Planting |
| L2 | Single Decorative Light |

The Streetscape Beautification Plan

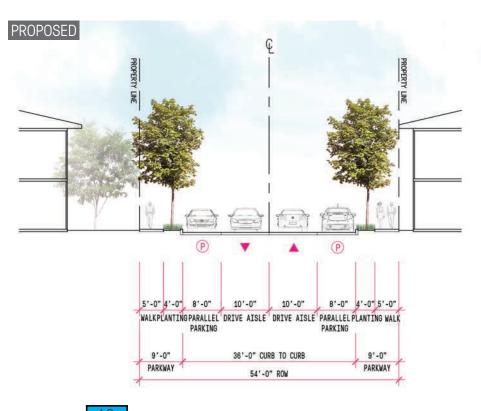
REVITALIZE UPTOWN

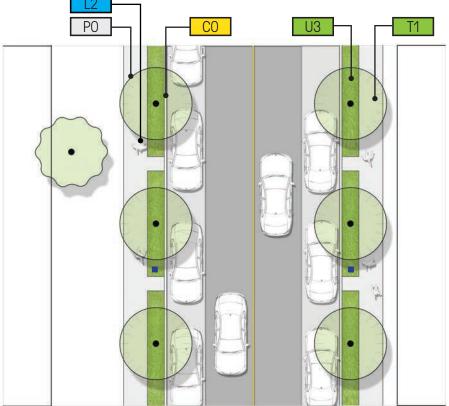
TYPE III STANDARD **50-55' ROW**

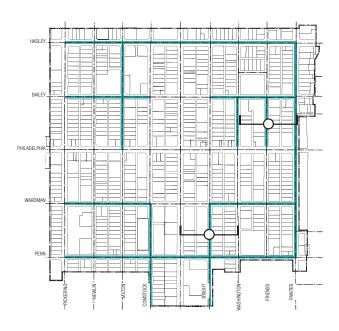


KEY DESIGN CONCEPT

Several north-south streets have 50-55' ROW which do not have adequate room for proper street tree planting. To create the best scenario for street trees, the planting strips should be made continuous to provide the largest area possible.







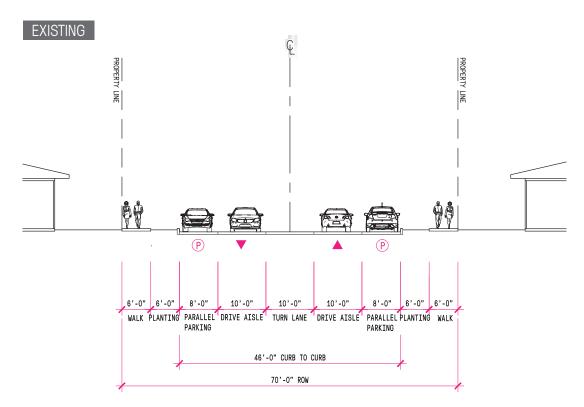
| PROPOSED STREET STANDARDS | |
|---------------------------|------------------------------------|
| VEHICULAR | |
| Right of Way | 50'-55' |
| Sidewalk Width | 5' Clear |
| Traffic Lanes | 1 Lane Each Way (10' Travel Lanes) |
| Parking Lanes | Parallel |
| Bike | Shared With Vehicular Traffic |
| LANDSCAPE | |
| Tree Location | Parkway |
| Tree Spacing | 30' Maximum Typ. |
| Tree Type | Canopy |
| Sustainable Strategies | n/a |

| DESIGN | DESIGN PALETTE | |
|--------|--|--|
| | ITEM | |
| P0 | Existing City Standard Concrete Paving | |
| CO | Existing Curb | |
| T1 | Canopy Tree | |
| U3 | Standard Planting | |
| L2 | Single Decorative Light | |



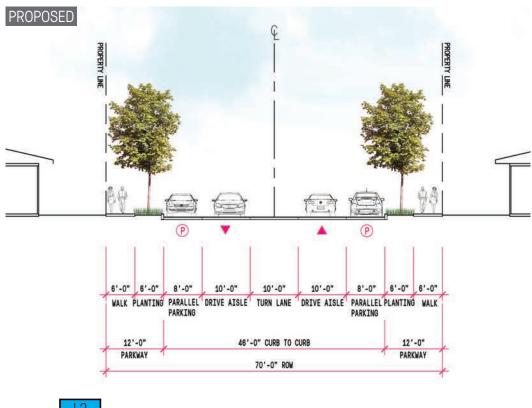
REFRESH UPTOWN

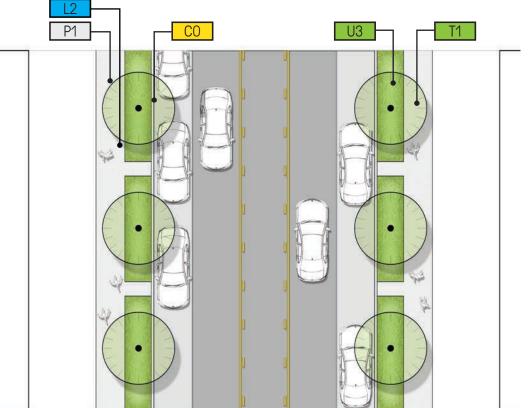
TYPE IV STANDARD 70' ROW | TURN LANE

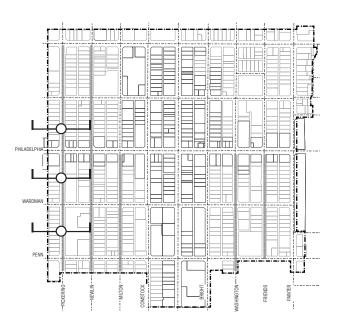


KEY DESIGN CONCEPT

Pickering has adequate room for street trees. No significant changes are proposed.







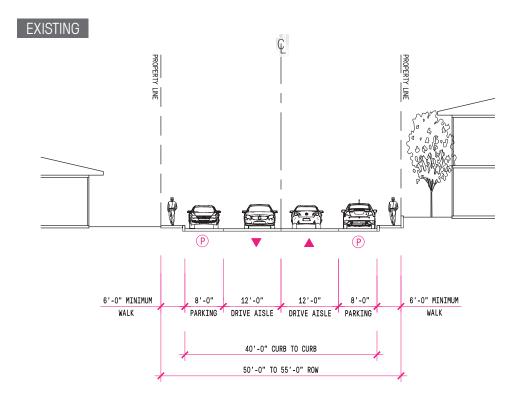
| PROPOSED STREET ST | PROPOSED STREET STANDARDS | |
|------------------------|--|--|
| VEHICULAR | | |
| Right of Way | 70' | |
| Sidewalk Width | 6' Clear | |
| Traffic Lanes | 1 Lane Each Way (10' Travel Lanes) And Center Turn Lane (10') | |
| Parking Lanes | Parallel | |
| Bike | Shared With Vehicular Traffic | |
| LANDSCAPE | | |
| Tree Location | Parkway | |
| Tree Spacing | 30' Maximum Typ. | |
| Tree Type | Canopy | |
| Sustainable Strategies | n/a | |
| | • | |

| DESIGN | PALETTE |
|--------|--|
| | ITEM |
| PO | Existing City Standard Concrete Paving |
| CO | Existing Curb |
| T1 | Canopy Tree |
| U3 | Parkway Planting |
| L2 | Single Decorative Light |

The Streetscape Beautification Plan

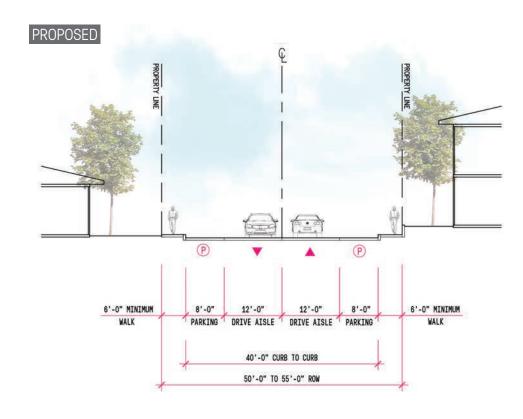
REFRESH UPTOWN

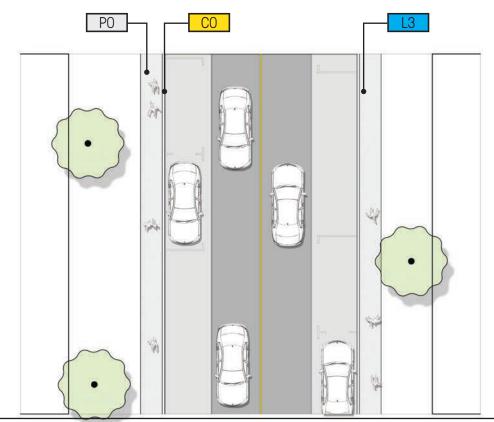
TYPE IV STANDARD **50'-55' ROW**

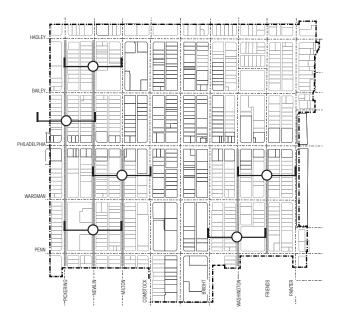


KEY DESIGN CONCEPT

North-South residential streets have a typical ROW between 50' and 55'. These streets do not contain adequate ROW for street tree planting. Trees should be encouraged in front yards.







| PROPOSED STREET STANDARDS | | |
|---------------------------|------------------------------------|--|
| VEHICULAR | | |
| Right of Way | 50'-55' | |
| Sidewalk Width | 6' Clear | |
| Traffic Lanes | 1 Lane Each Way (10' Travel Lanes) | |
| Parking Lanes | Parallel | |
| Bike | Shared With Vehicular Traffic | |
| LANDSCAPE | | |
| Tree Location | Private Yard | |
| Tree Spacing | n/a | |
| Tree Type | n/a | |
| Sustainable Strategies | n/a | |

| DESIGN | DESIGN PALETTE | |
|--------|--|--|
| | ITEM | |
| P1 | Existing City Standard Concrete Paving | |
| CO | Existing Curb | |
| L3 | Standard Roadway Light | |



| S | TREET | SCAP | E FAI | MILIES |
|---|--------------|------|-------|--------|
|---|--------------|------|-------|--------|

REFRESH UPTOWN

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KEY **Nodes**

OVERVIEW

Several types of key nodes exist within Uptown Whittier, highlighting distinct areas within the district including the primary intersections of Greenleaf and Philadelphia, other core area intersections, gateways, mid-block crossings, parking planters and parklets.









KEY MAI



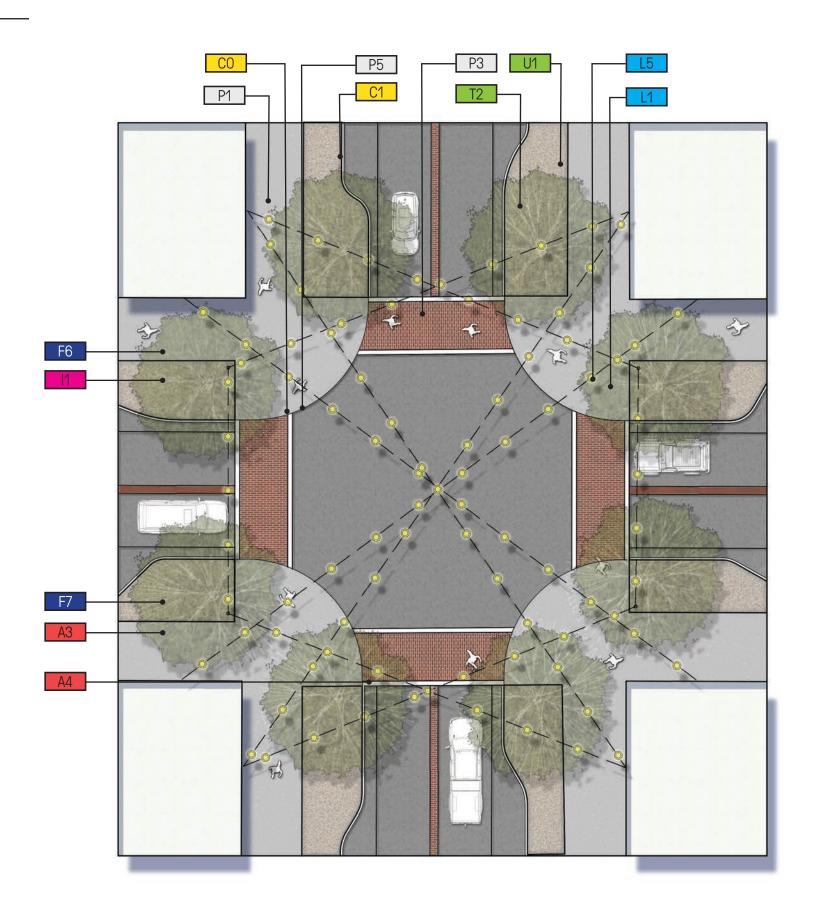
KEY NODES

INTERSECTIONS



KEY DESIGN CONCEPTS

Intersection Type A is at the primary intersection of Greenleaf and Philadelphia. It should be the most celebrated intersection in Uptown Whittier, including specialized lighting and public art installations.





| DESIGN | PALETTE |
|--------|-------------------------------|
| | ITEM |
| P1 | City Standard Concrete Paving |
| P3 | Paver |
| P5 | Truncated Domes |
| CO | Existing Curb |
| C1 | New Curb |
| F6 | Bike Parking |
| F7 | Waste Receptacle |
| T2 | Accent Tree |
| U1 | Garden Planting |
| L1 | Double Decorative Light |
| L5 | Specialized Lighting |
| 11 | Public Art |
| A3 | Parking Meter |
| | |

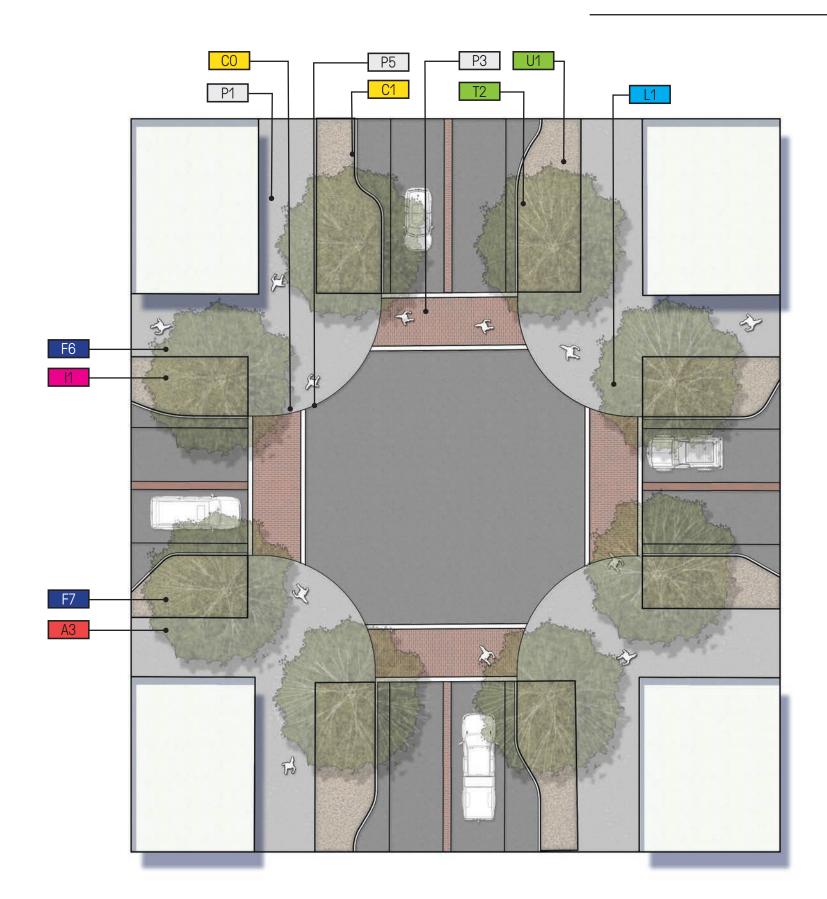


INTERSECTIONS



KEY DESIGN CONCEPTS

Intersection Type B is at the secondary intersections along Greenleaf and Philadelphia, one block away from Intersection Type A. This intersection gets specialized paving material similar to Intersection Type A.





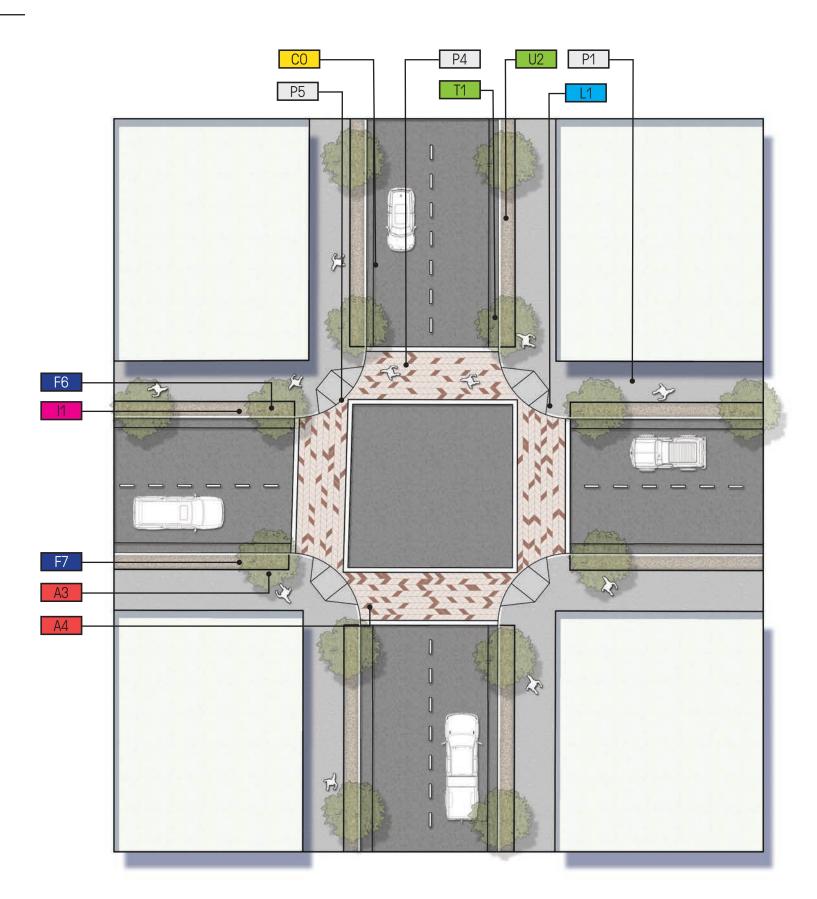
| ESIGN PALETTE | | | |
|---------------|-------------------------------|--|--|
| | ITEM | | |
| P1 | City Standard Concrete Paving | | |
| P3 | Paver | | |
| P5 | Truncated Domes | | |
| CO | Existing Curb | | |
| C1 | New Curb | | |
| F6 | Bike Parking | | |
| F7 | Waste Receptacle | | |
| T2 | Accent Tree | | |
| U1 | Garden Planting | | |
| L1 | Double Decorative Light | | |
| 11 | Public Art | | |
| A3 | Parking Meter | | |
| | | | |

INTERSECTIONS



KEY DESIGN CONCEPTS

Intersection Type C is a tertiary intersection at the edge of the core of Uptown Whittier. In includes crosswalks of artful painted striping and public art installations to distinguish it from regular intersections.





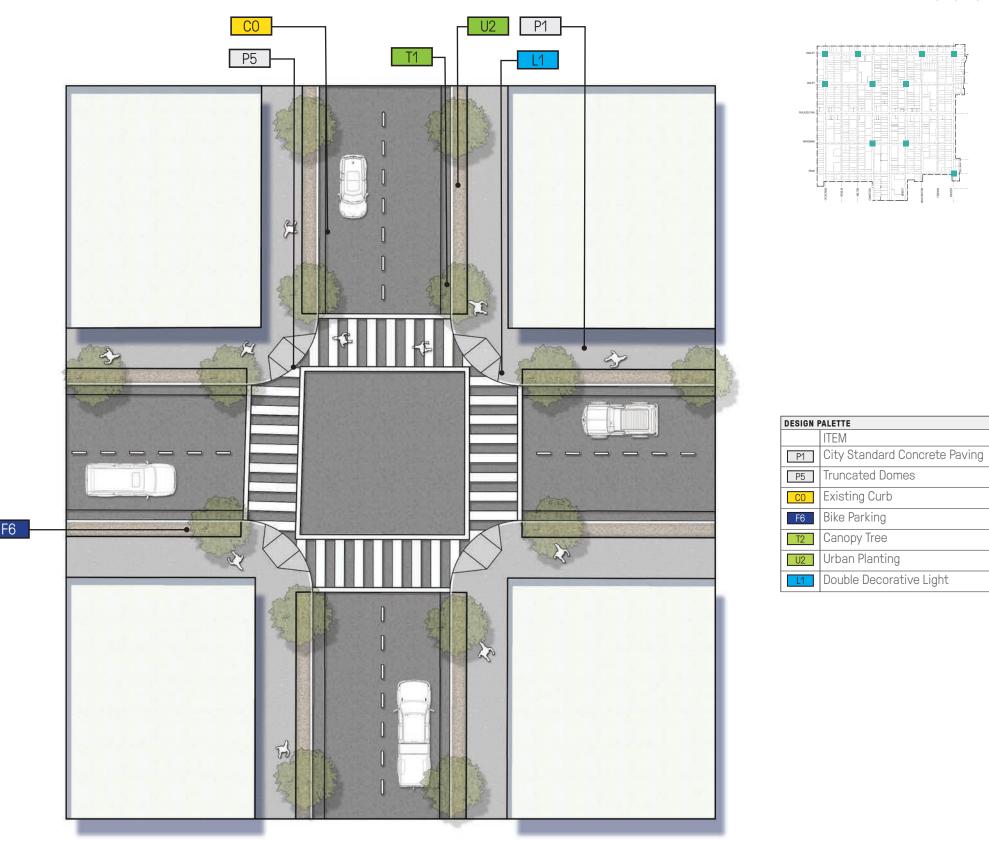
| DESIGN PALETTE | | | |
|----------------|-------------------------------|--|--|
| | ITEM | | |
| P1 | City Standard Concrete Paving | | |
| P4 | Artful Painted Striping | | |
| P5 | Truncated Domes | | |
| CO | Existing Curb | | |
| F6 | Bike Parking | | |
| F7 | Waste Receptacle | | |
| T2 | Canopy Tree | | |
| U2 | Urban Planting | | |
| L1 | Double Decorative Light | | |
| 11 | Public Art | | |
| A3 | Parking Meter | | |
| A4 | Removable Bollards | | |

INTERSECTIONS



KEY DESIGN CONCEPTS

Intersection Type D is a standard intersections along Philadelphia. As a higher traffic pedestrian area, crosswalks in this intersection have a greater graphic presence.



KEY NODES

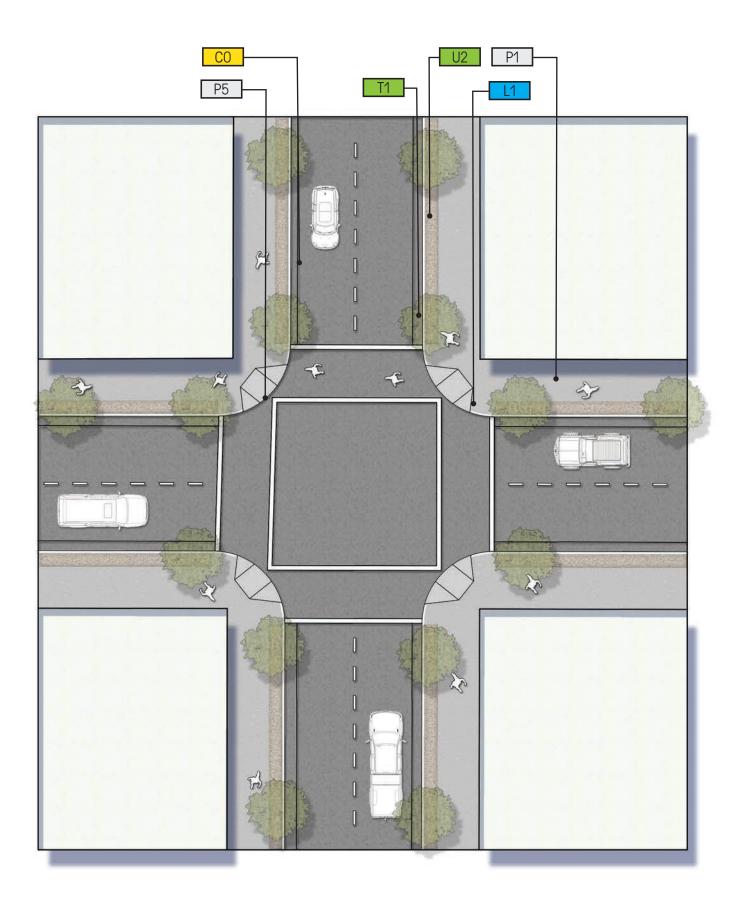
INTERSECTIONS



INTERSECTION E STANDARDS

KEY DESIGN CONCEPTS

Intersection Type E is a standard intersection in Uptown Whittier.





| DESIGN PALETTE | | |
|----------------|-------------------------------|--|
| | ITEM | |
| P1 | City Standard Concrete Paving | |
| P5 | Truncated Domes | |
| CO | Existing Curb | |
| F6 | Bike Parking | |
| T2 | Canopy Tree | |
| U2 | Urban Planting | |
| L1 | Double Decorative Light | |

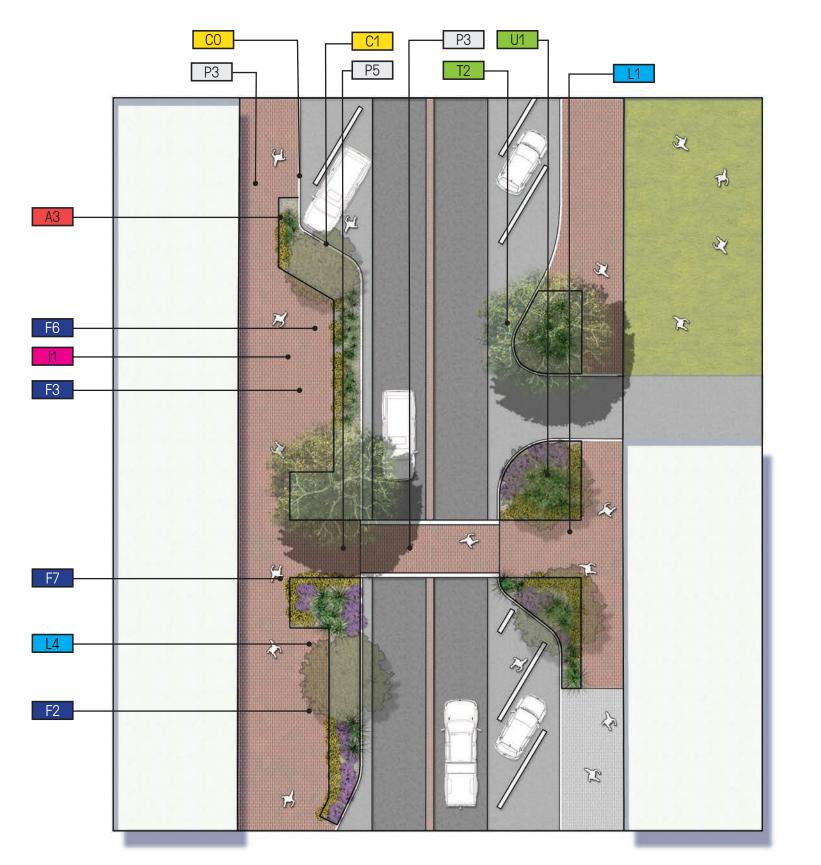


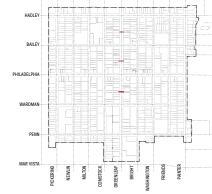
MID-BLOCK CROSSINGS



KEY DESIGN CONCEPTS

Mid-block crossings occur along Greenleaf, two to the north of Philadelphia and one to the south. Mid-block crossings general connect paseos and aid in providing access to parking lots or structures located on the far side of the blocks. Mid-block crossings also help slow traffic, provide safer places for pedestrians to cross the street and allow for added room for street furnishings.





| ESIGN PALETTE | | | |
|---------------|-------------------------|--|--|
| | ITEM | | |
| P3 | Paver | | |
| P5 | Truncated Domes | | |
| CO | Existing Curb | | |
| C1 | New Curb | | |
| F2 | Group Seating | | |
| F3 | Specialized Seating | | |
| F6 | Bike Parking | | |
| F7 | Waste Receptacle | | |
| T2 | Accent Tree | | |
| U1 | Garden Planting | | |
| L1 | Double Decorative Light | | |
| L4 | Festoon Lighting | | |
| 11 | Public Art | | |
| A3 | Parking Meter | | |
| | | | |

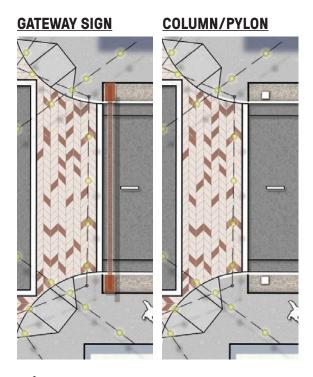
GATEWAYS

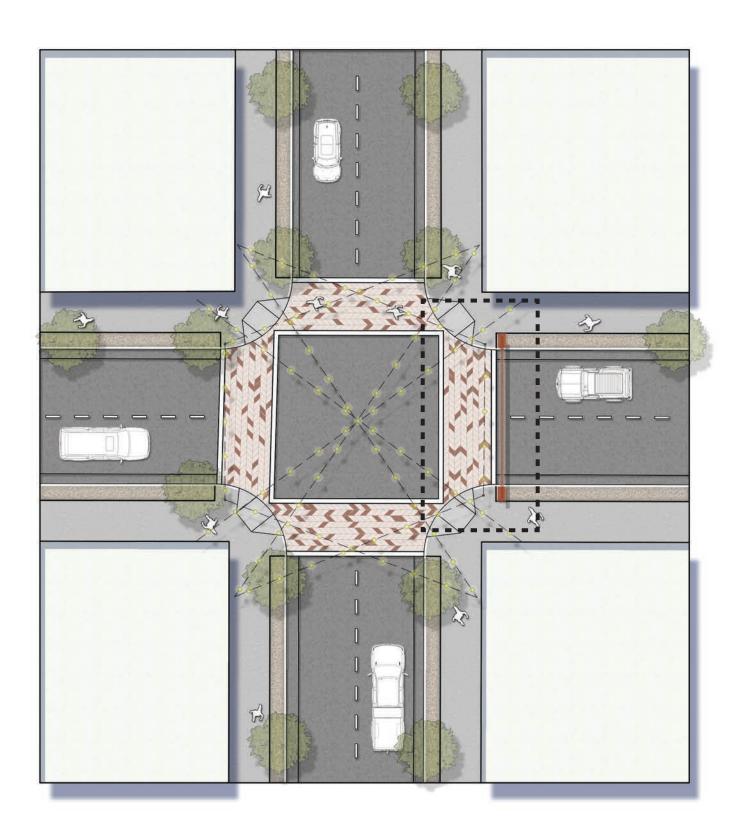
GATEWAY Standards

KEY DESIGN CONCEPTS

Gateway intersections are exceptional locations where it is important to highlight the entryways into Uptown Whittier and the edges of this district. These intersections shall be enhanced with a coordinated program of components including at least three of the following:

- Pedestrian lighting/ other specialty lighting.
- Columns/pylons, gateway sign or other urban design structures.
- Signal or light pole treatments.
- Special paving or intersection treatment.
- Sculpture or other public art installation in addition to the components listed above.

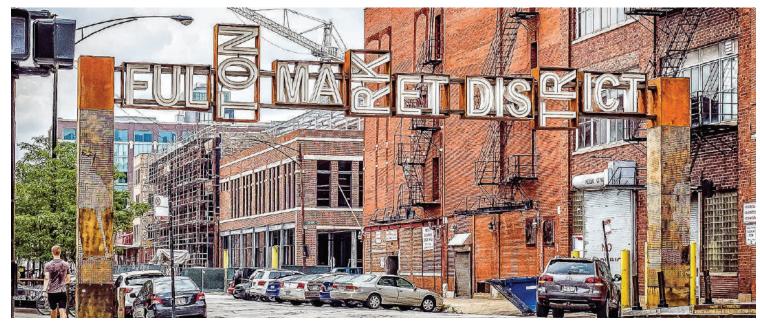




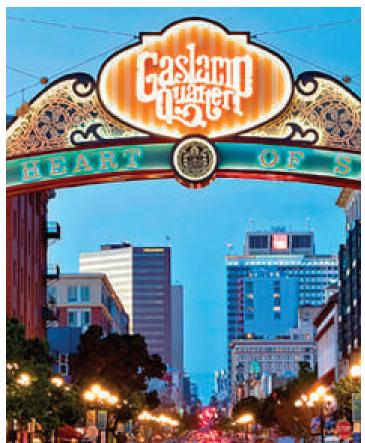


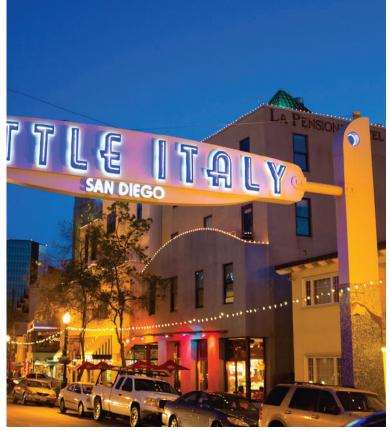


OPTION A OPTION B













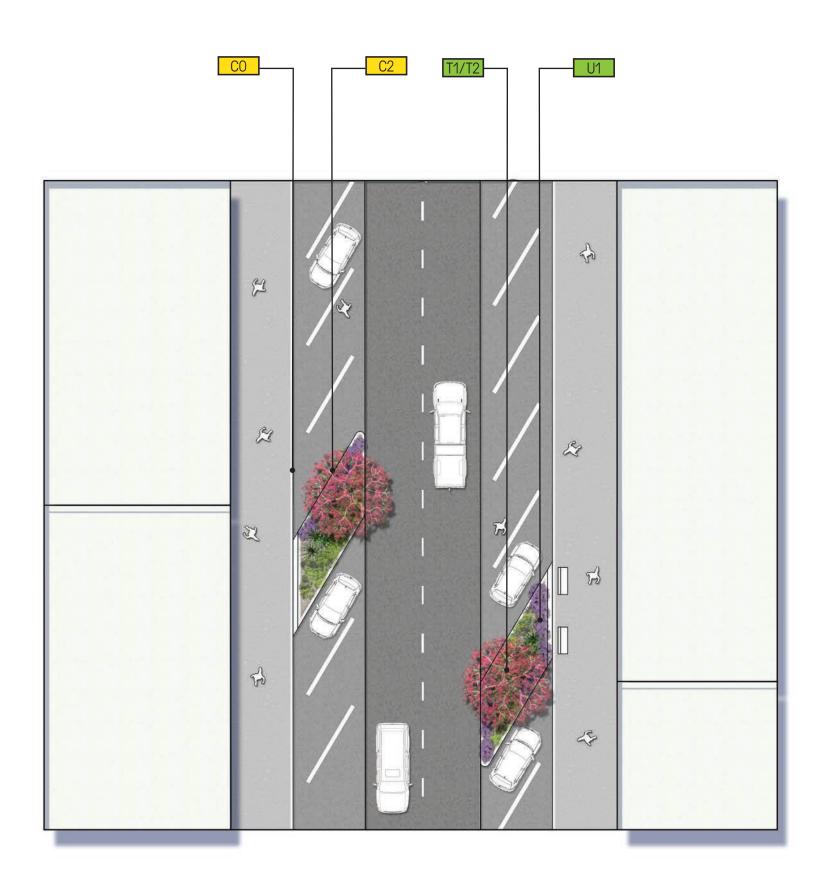
PARKING PLANTER

PARKING PLANTER STANDARDS

KEY DESIGN CONCEPTS

Parking planters are landscaped sidewalk extensions placed between or instead of parking spaces at regular intervals or at specific locations. They provide space for street trees and landscaping on streets with narrow sidewalks, where tree planting is limited by conflicts with utilities or driveways, or where there is a desire to visually narrow the roadway.

Parking planters are the size of a parking space and may reduce the number of on-street parking spaces so should be studied to ensure that enough onstreet parking is provided.



| DESIGN PALETTE | | |
|----------------|-----------------------|--|
| | ITEM | |
| CO | Existing Curb | |
| C2 | Parking Planter Curb | |
| T1/T2 | Canopy or Accent Tree | |
| U2 | Garden Planting | |

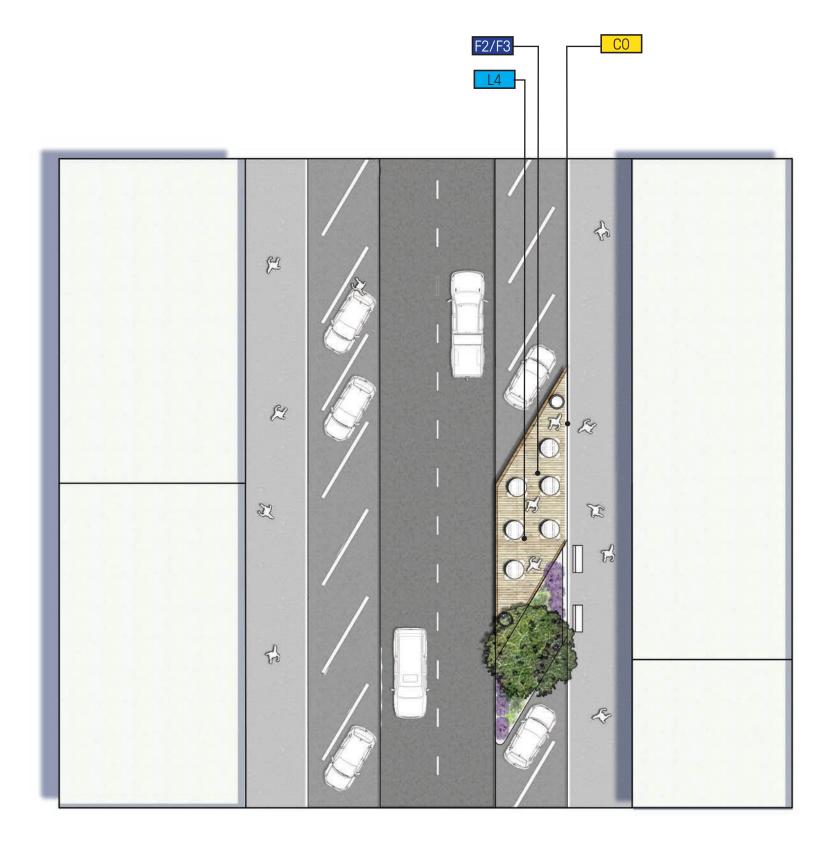




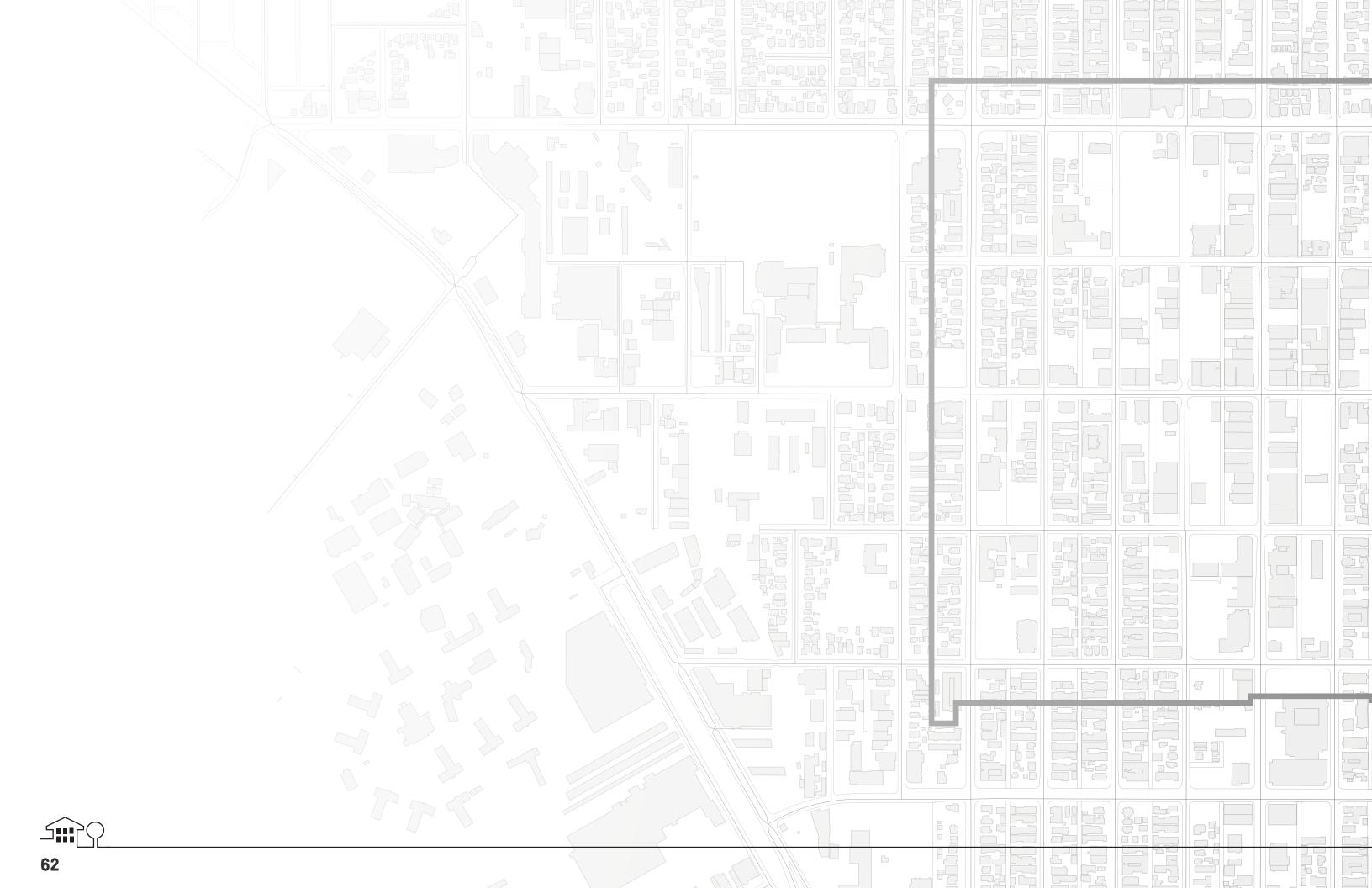
KEY DESIGN CONCEPTS

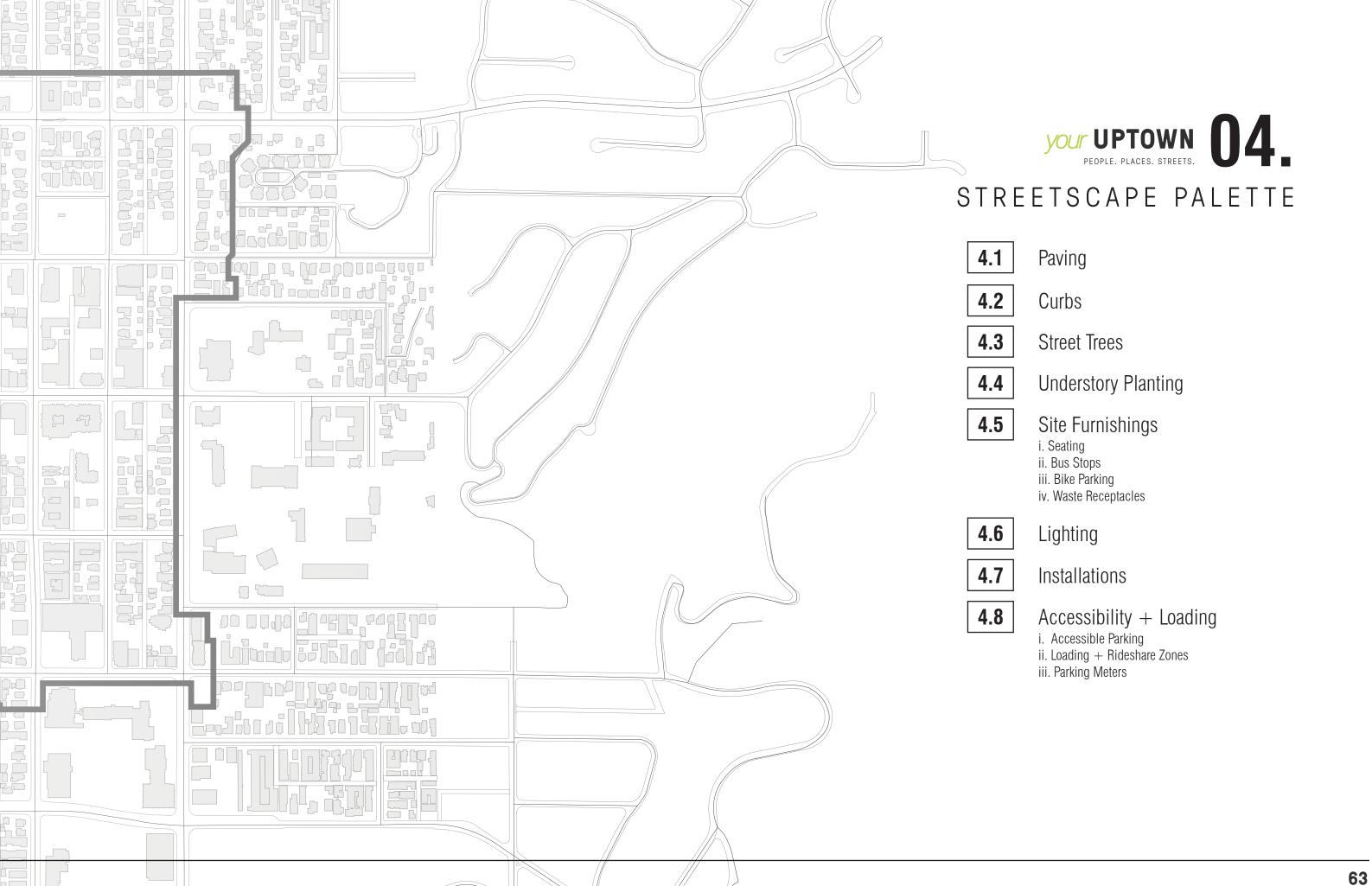
Parklets provide an economical solution to the desire and need for wider sidewalks and are intended to provide space for the general public to sit and enjoy the space adjacent to existing narrow sidewalks. Parklets are intended as sidewalk/street furniture for public use, providing aesthetic elements to the overall streetscape, with care by adjacent businesses and residents.

Parklets are typically the size of two parking spaces. Each parklet shall provide an appropriate perimeter fence or barrier that is fitting to the design aesthetic of the parklet and are intended to provide security from neighboring vehicular paths of travel.



| DESIGN PALETTE | | | |
|----------------|---------------------|--|--|
| | ITEM | | |
| CO | Existing Curb | | |
| F2 | Group Seating | | |
| F3 | Specialized Seating | | |
| L4 | Festoon Lighting | | |





PAVING Standards

The following paving materials shall be used in sidewalks throughout Uptown Whittier. All sidewalk paving must be durable and practical from a maintenance standpoint and safe and comfortable for all pedestrians, including those in wheelchairs.

Paving materials contribute to the character of a street. In addition to standard concrete, special paving shall be used judiciously to enhance the quality and define sidewalk zones or at street crossings along several key intersections.

P0



EXISTING CONDITION - CITY STANDARD CONCRETE PAVING, NATURAL GRAY

Color, finish and condition of existing concrete paving varies. There are signs of cracking, lifting and fading present throughout the site and in need of renovation.

P1



CITY STANDARD CONCRETE PAVING, NATURAL GRAY

Refer to City of Whittier standard paving detail for color, type, finish and joint layout of pedestrian concrete paving.

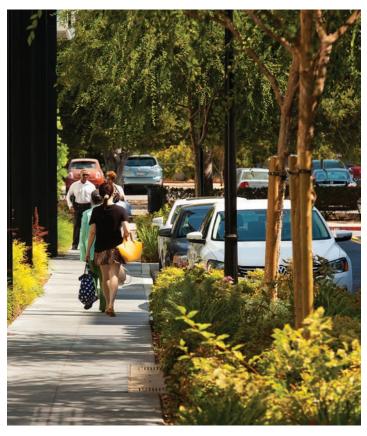
P2



ENHANCED CAST IN PLACE CONCRETE

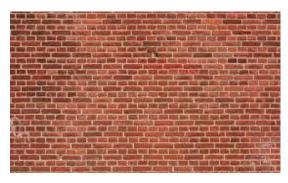
Enhanced concrete may have an exposed aggregate finish for a rich, textured surface and may incorporate special joint patterns for a more refined appearance. Integral color and decorative aggregates shall be selected for an elevated aesthetic quality and to meet accessible design requirements for slip resistance.











PAVER

Solid, interlocking brick or concrete paving modules with distinct patterns will be installed to indicate pedestrian and vehicular traffic delineation, extents of parking spaces and areas of slower traffic. Modules with consistent color, size and finish shall be specified to maintain a homogeneous appearance throughout the site. All modules shall be selected to meet slip resistance requirements outlined per the CBC and minimum thickness required to withstand vehicular load.

P4



ARTFUL PAINTED STRIPING

Paint will be used to establish crosswalks and lane markings that indicate separation of a two-way vehicular traffic. Vehicular traffic striping shall be painted yellow or white. Pedestrian crosswalk striping colors shall vary as specified by the landscape architect.

P



TRUNCATED DOMES - ADA DETECTABLE PAVING

Used where pedestrians enter vehicular zones of the street, detectable paving clearly delineates the edge or end of the pedestrian-only zone with a textured surface, such as approved truncated dome products. Material shall meet accessible design requirements for slip resistance and provides high visual contrast (70% from adjacent paving).

CURB **Standards**

Curbs, gutters, and curb ramps shall comply with City standards.

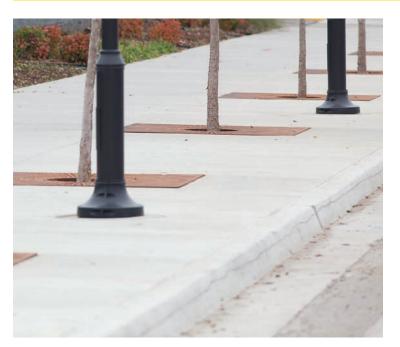
CO



EXISTING CURB

Existing curbs shall remain as is unless otherwise directed by the City.

C1



NEW CURB

New curb to City standard. New curb designs must be reviewed and approved by the City.







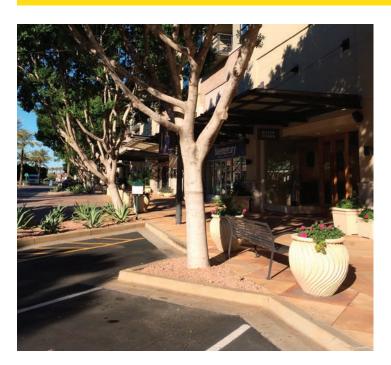
C2



PARKING PLANTER CURB

Curbs at on-street parking planters shall allow existing draingage patterns to continue by either offsetting new curbs from existing curbs by a minimum of 1' or providing curb cuts to allow draingage through the planter areas. If feasible, the latter of these methods is preferred.

C



DIAMOND PLANTER CURB

Diamond planters extend the available area to plant trees into areas of angled parking spaces.

Diamond planter curbs should allow existing drainage patterns to remain in place, providing curb cuts to let water in and out of the planter.

STREET TREES STANDARDS

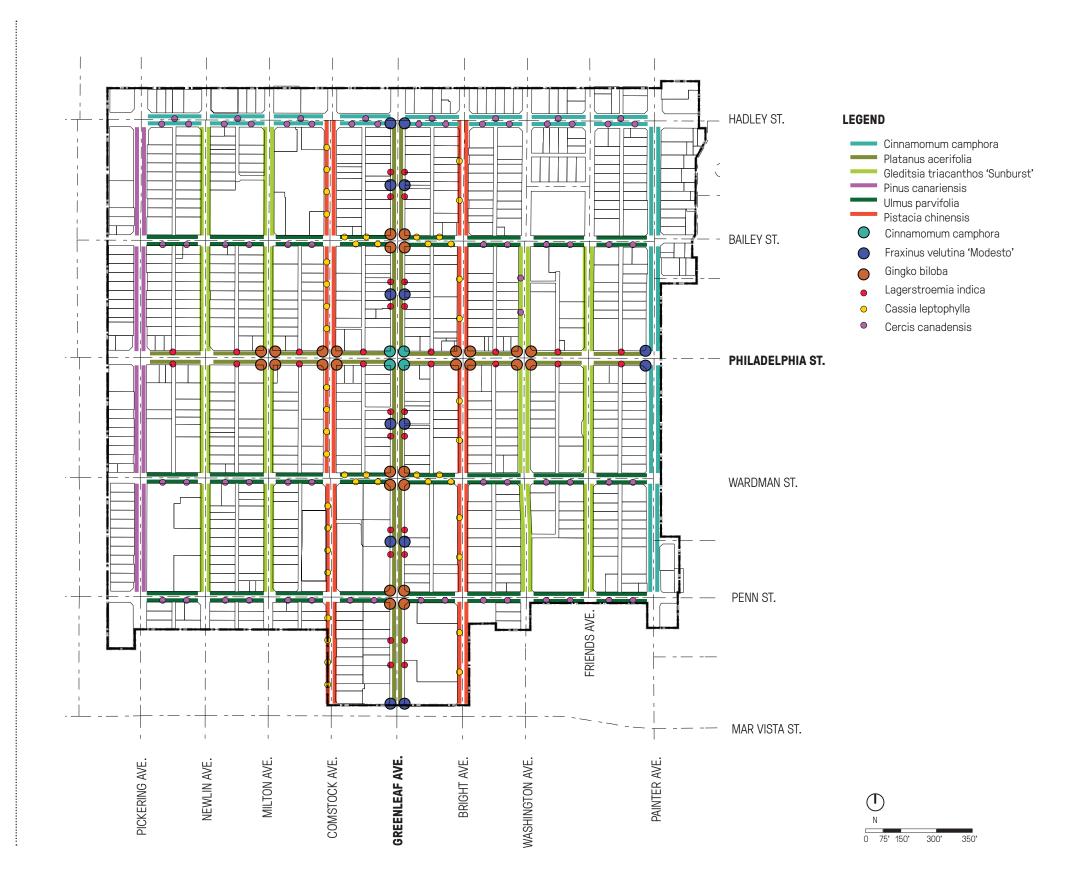
As street trees are some of the most functional and iconic elements in the streetscape, careful selection is important in creating a successful urban forest. Species selection criteria for Uptown Whittier include:

- Species adapted to the Uptown Whittier micro-climate and soil conditions
- Trees that thrive receiving the natural regional precipitation and require minimal supplemental irrigation
- Species that have proven long-term durability in the region
- Species that are tolerant of rough urban conditions and are adaptable to adjacencies to development
- Although not required, trees that provide habitat opportunities for birds and other small wildlife are encouraged

A collection of street trees has been identified for their character and potential to thrive in Uptown Whittier. These trees have been divided into two categories; Canopy trees that define the overall character of a street and Accent trees that are located to highlight specific parts of a street such as intersections, paseos or mid-block crossings.

The collection of canopy trees have been chosen based on the Uptown Whittier Specific Plan and discussions with the city.

Stree tree designations are meant to provide guidance as new trees are planted or aging trees are replaced. Healthy, mature trees that do not pose a safety or maintnence risk should remain in place. For example, the Ficus Trees have been deemed a safety and maintenance risk and should be replaced per the Uptown Whittier Specific Plan.





T1

STREET CANOPY TREE

LONDON PLANE TREE | PLATANUS X ACERIFOLIA 'COLUMBIA'

TYPE: Canopy

MATURE SIZE: 70-80' H / 40-50' W

WATER USE: Lov

Platanus x acerifollia 'Columbia' is a hybrid version of the London Plane that is resistant to anthracnose and mildew as is common with other Platanus species and varieties. The upright, rounded growth habit are study in wind conditions and are adapted to poor soil conditions and city conditions. Columbia Plane Trees are deciduous and require full sun exposure and are low water using trees. The striking branching and sturdy rooting make it a great tree for the character road in Uptown Whittier.



THORNLESS HONEY LOCUST | GLEDITSIA TRIACANTHOS INERMIS 'SUNBURST'

TYPE: Canopy

MATURE SIZE: 50-70' H x 20-30' W

WATER USE: Low

The thornless honey locust is a fast growing canopy tree that is strong wooded and features yellow/gold fall color. The inermis variety is both thornless and nearly seedless. A nice option for urban locations as it is tolerant to drought and air pollution.



CHINESE PISTACHE | PISTACIA CHINENSIS

TYPE: Canopy

MATURE SIZE: 25-35' H / 25-35' W

WATER USE: Los

Medium to large deciduous ornamental street tree with an oval, rounded shape and striking fall foliage. Grows well in full sun and can withstand low water conditions, heat and urban conditions well.



CAMPHOR TREE | CINNAMOMUM CAMPHORA

TYPE: Canopy

MATURE SIZE: 50-65' H / 50-60' W

WATER USE: Medium

Cinnamomum camphora is a large evergreen shade tree that thrives in full sun and require low water once established. A nice option for urban conditions with strong branching habits and a erect, rounded canopy shape.



CHINESE ELM | ULMUS PARVIFOLIA

YPE: Canopy

MATURE SIZE: 40-60' H / 50-70 W

WATER USE: Mediu

Ulmus parvifolia is a semi-evergreen tree that has a wide, natural umbrella shaped form with a striking bark color and leaf structure. The Chinese Elm is a fast growing species that is suited for full sun and has medium water needs.



CANARY ISLAND PINE | PINUS CANARIENSIS

TYPE: Canopy

MATURE SIZE: 50-80' H / 20-35' W

WATER USE: Lo

Pinus canariensis is a tall narrow evergreen shade tree that requires ample growth space. It is dense in leaf and provides moderate shade along a streetscape with its narrow structure.

STREET TREES

CANOPY

STREET ACCENT TREE

MODESTO ASH | FRAXINUS VELUNTINA 'MODESTO'

TYPE: Accent

MATURE SIZE: 30-50' H / 30-40' W

WATER USE: Medium

A deciduous shade tree with a rich green color that turns golden yellow in fall. Once established the Modesto Ash is drought tolerant and requires minimal irrigation needs. This large and fast growing variety is cold, heat and smog tolerant.



PINK TRUMPET TREE | HANDROANTHUS IMPETIGINOSA

TYPE: Accent

MATURE SIZE: 20-30' H / 15-20 W

WATER USE:

Deciduous with a branching canopy, often large but sparse. Pink trumpet-shaped flowers bloom in spring, providing a spectacular accent tree.



WESTERN REDBUD | CERCIS OCCIDENTALIS

Accent

MATURE SIZE: 10-20' H / 10-20 W

WATER USE:

Cercis occidentalis is a California native deciduous tree features striking magenta fall flowers. Commonly found as a multi-trunk species. Tolerates dry conditions as well as full sun to partial shade.



MAIDEN HAIR TREE | GINKGO BILOBA

TYPE: Accent

MATURE SIZE: 35-65' H / 25' W Medium

WATER USE:

A notable, distinct deciduous tree that features striking fall leaf color and fan shaped leaves. As it is easy to establish and can withstand many urban conditions (most notable air potion, heat, and confined spaces) it is an ideal canopy or ornamental tree. Male varieties reduces fragrant fruit litter that is unpleasant.



CRAPE MYRTLE | LAGERSTROFMIA INDICA x FAURIFI 'MUSKOGFF'

Accent

MATURE SIZE: 20-30' H / 15-20 W

WATER USE: Medium

Lagerstroemia indica x fauriei 'Muskogee' is a hybrid that features striking, long periods of summer flower colors and an attractive bark and foliage. These species are highly mildew resistant, drought tolerant and grows well in a limited soil space. Foliage is dark green and changes to warm shades during fall months. Well suited for hot climates and grows quickly.



STRAWBERRY TREE | ARBUTUS UNEDO

Accent

MATURE SIZE: 20-35' H / 20-35' W

WATER USE:

An attractive specimen tree that is evergreen in nature that bears edible fruit. The natural rounded shape and attractive red-brown bark has strong branch strength with low root damage potential.



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UNDERSTORY PLANTING STANDARDS

Understory plantings enhance the identity of a street network and provide opportunities for uniquely characterizing different areas within Uptown Whittier. The following palette represents an array of locally-adapted species both native to the area and suitable to Mediterranean climates, noted for their interesting form, flower, foliage, and urban resilience. General requirements for understory planting species are as follows:

- Compatibility with site soils
- Durability in urban settings
- Low water usage
- Compatibility with corresponding street trees
- Low maintenance
- Serving the street service needs (such as bio-filtration)
- Seasonal interest
- Ecological benefits

These general guidelines shall apply to future species selections. However, the plant palettes highlighted in this document express a design intention, and selection of the plants is to be determined within the design of each block.





1 GARDEN PLANTING

Garden Planting is reflective of a historic Whittier and special use zones. These areas are programmed for high pedestrian activity. Vibrant in color and texture, shrubs and groundcovers colorfully contribute to a buzzing urban realm.

2 URBAN PLANTING

Located in Urban tree planters. Low-growing groundcovers that can withstand high foot traffic or mulch are most suitable for these urban conditions.

STANDARD PLANTING

Standard Planting is along parkways adjacent to residential frontage. Planted areas extend through the neighborhoods of Uptown.

SHRUBS | GROUNDCOVERS



BLACK SAGE SALVIA MELIFERA



COFFEE BERRY



BLONDE AMBITION BUTULA XX



BLUE EYED GRASS

SHRUBS | GROUNDCOVERS



MEXICAN FEATHER GRASS STIPA TENUISSIMA



PENNISETUM SPATHIOLATUM



LANTANA



YERBA MANSA ANEMOPSIS CALIFORNICA SHRUBS | GROUNDCOVERS



MULCH



BERMUDA GRASS CYNODON DACTYLON



FRAGARIA CALIFORNICA WOOD STRAWBERRY



BUGLEHERB AJUGA REPTANS SHRUBS | GROUNDCOVERS



BERMUDA GRASS CYNODON DACTYLON

SITE FURNISHING STANDARDS

OVERVIEW

Site furnishings within Uptown Whittier primarily include various types of seating as well as bus stops, bike parking and waste receptacles. Seating on streets provides a place for people to stop and rest, have a conversation, or eat a meal. Seats—benches, chairs and other objects that provide a surface for sitting—are important because they make streets more humane: inviting, comfortable, and attractive.

The Uptown Whittier approach to seating includes several types of seating to be incorporated into the design of streets. These include a standard bench throughout Uptown Whittier in addition to special seating types for the different levels of streetscape improvements (Type I, II and III). Each type is specially suited to its situation and supports the unique identity of the surrounding neighborhood. Residential streets do not have a seating standard.

Together these seating types, along with bus stops, bike parking and waste receptacles, provide a basic level of service for encouraging robust pedestrian use of the Uptown Whittier streetscapes.

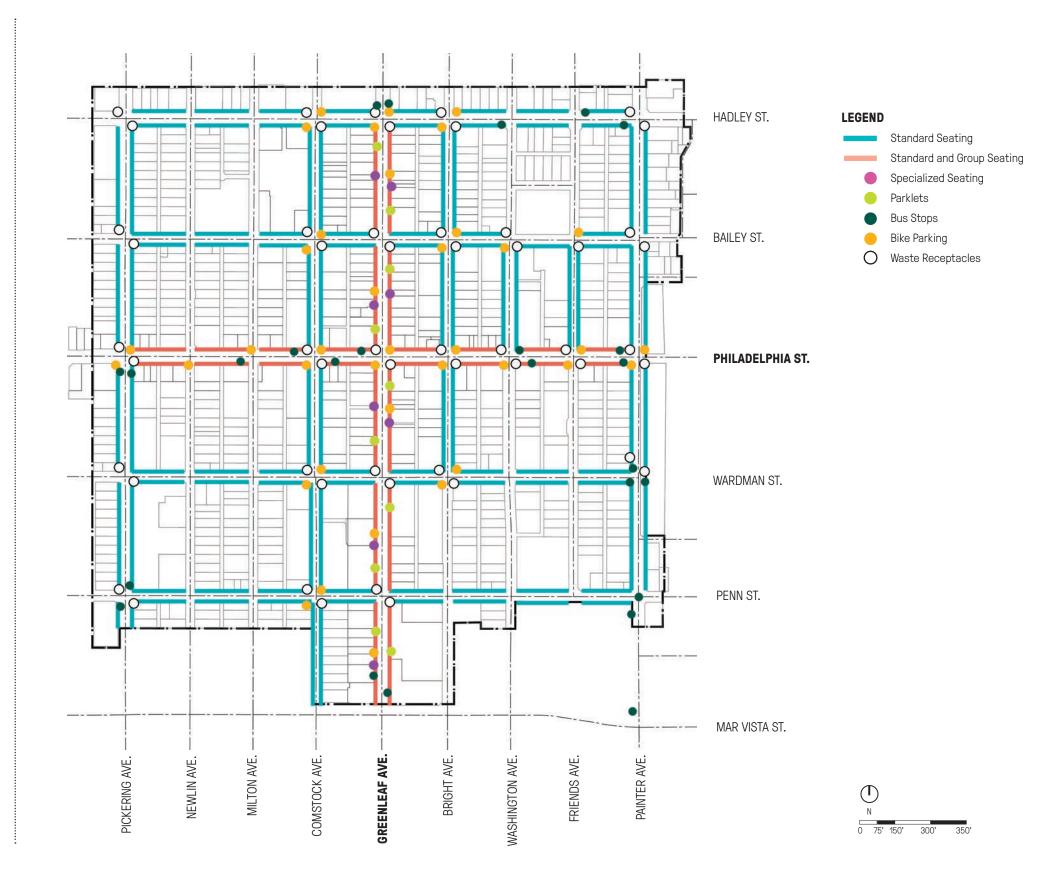
DESIGN GUIDELINES + CONSIDERATIONS

Furnishings should be selected or designed to be:

- Inviting, comfortable, and accessible to all people.
- Family friendly.
- Attractive, and support the value of high quality in the public realm.
- Durable and resilient, suitable for high-use in an urban environment.

Furnishings should be located where:

- They are most likely to be used: where they are visible, convenient, and comfortable, or where they provide an appealing view to users.
- They are part of an inviting arrangement of streetscape features such as planting areas trees, or other seats.
- They are outside the sidewalk's path of travel, there is sufficient space for comfortable sitting (i.e. leg room), and the functions of the sidewalk are not compromised by the furnishing.
- Furnishings shall provide deterrent hardware to prevent skateboarding.





STANDARD SEATING









The Uptown Whittier Standard bench shall be located throughout the Uptown's streets to ensure a basic level of seating everywhere, so that anyone can rely on regular places to stop and rest briefly, on any pedestrian route within Uptown.

As a recurring feature of Uptown Whittier's streetscapes, the bench Standard is a unifying design element that expresses the value of high quality in the public realm.

DESIGN CRITERIA

- Contemporary and timeless in style.
- Comfortable for short stays but not intended for extended use.
- Optional backrests and armrests.
- Areas with backless benches without armrests shall also include at least one bench with back and armrests.
- Each seating area shall also allow for wheelchair user seating positioned in side by side, shoulder-to-shoulder configuration.
- Suitable for a range of sidewalk conditions.

- On Type I, II and III Streets.
- Comfortable, inviting positions associated with other streetscape features.
- Regular intervals, to provide a baseline level of reliable place to stop and rest along pedestrian routes.
- Sidewalk furnishing zones and bulb-outs, especially at corners.
- Furnishings shall provide deterrent hardware to prevent skateboarding.

GROUP SEATING

GROUP SEATING

In the Core Area of Uptown, Group Seating provides social seating arrangements for neighbors and visitors: places to have a conversation or eat lunch. A suite of distinctive, matching furniture pieces—such as love seats, comfortable chairs, and coffee tables—invites people to spend time in the street and also coheres the length of several, eclectic blocks with a standard, special group of furnishings.

DESIGN CRITERIA

- A suite of distinctive matching furniture pieces: chairs, benches, and tables
- Comfortable for leisurely use, such as for reading a book or eating lunch.
- Strong, consistent image: contemporary and timeless in style.

- On Type I and II Streets.
- Consistently located throughout the entirety of the Core Area.
- In comfortable, social arrangements where the ROW permits, especially at corners.











SPECIALIZED SEATING









The Uptown Whittier seating program is enhanced by additional seating elements that are designed specifically for Type I Streets, to support a distinct image and character that enriches the very center of Uptown and set it apart from other streets. This supplemental seating is integrated into the design of the street.

DESIGN CRITERIA

- Consistent through the length of the street.Rare quality and clear design intention for comfort, use, and unique identity.
- Comfortable for leisurely and social use.
- Furnishings shall provide deterrent hardware to prevent skateboarding.

- On Type I Streets.
- Sunny, social locations.
- Near midblock crossings, paseos and neighborhood parks.

PARKLETS

PARKLETS

Parklets are special, comfortable outdoor "rooms" for shoppers and other visitors to spend time: pleasant places to be on the street -- to watch people or meet friends.

Parklets are clearly defined spaces, located outside the flow of pedestrian traffic. They provide a comfortable barrier from passing cars and integrated seating, but are also open and flexible, to invite use and adoption by adjacent businesses and vendors.

DESIGN CRITERIA

- Inviting for different types of use, including long stays.
- Spacious and flexible for varied uses: fixed elements with options for
- Provides a comfortable feeling of separation from passing cars. Visually open but well defined and distinct from the rest of the sidewalk.
- Rare quality and clear design intention for comfort, use, and unique
- Each parklet may be designed to have its own special use and character.
- Each parklet shall provide an appropriate perimeter fence or barrier that is fitting to the design aesthetic of the parklet and are intended to provide security from neighboring vehicular paths of travel.

- On Type I Streets
- Max (2) locations per side of any block, max (4) locations along any given block.
- Corresponding to adjacent storefronts or active building entrances.





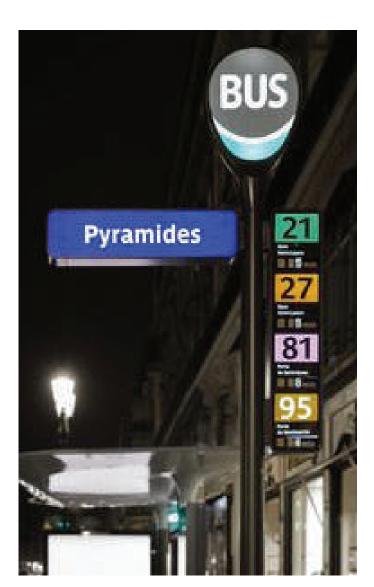






BUS STOPS





To encourage the use of public transportation in Uptown Whittier, an important component of the streetscape are bus stops. Bus stops should be supported by the design of the streetscape to provide ease of use for public transportation.

DESIGN CRITERIA

- Incorporate seating elements
- Include map/route information
- Are universally accessible
- Include lighting and standard security features

- Located in a bulb-out or furnishing zone.
- Do not impede into the ADA standards/ pedestrian throughway zone.
- Maintain required setbacks per City code from the curb.
- Placed at front zone and in a location that maximizes passenger flow.
- Bus stop bench furnishings shall provide deterrent hardware to prevent skateboarding.

BIKE PARKING

BIKE PARKING

Bike parking promotes bicycle use in Uptown Whittier for transportation and recreation. To provide secure short-term parking and express a commitment to cyclist and bike culture, high quality racks shall be located throughout the core of Uptown on all Type I, II and III Standard streets. Groups of two to three bike racks (for four to six bikes) shall be installed where needed.

DESIGN CRITERIA

- Racks should be strong, with at least two points of contact and multiple locking options for a range of bikes.
- Racks should be robust and well suited for a high-use urban environment and minimal maintenance.
- Racks should be consistent and distinctive to Uptown Whittier, refined and timeless in their design.

LOCATION CRITERIA

• On Type I, II and III Streets.

Individual bike racks or groupings on sidewalks should be located where:

- They are visible to cyclists and pedestrians.
- They will be well used.
- There is ample space and they do not compromise pedestrian flow or any other sidewalk function.
- They do not conflict with access to underground utilities.











WASTE RECEPTACLES









Uptown Whittier waste stations contribute to keeping streets clean and promoting good behavior by people who use them. Their frequent presence ensures a place to put one's waste and exhibits a value of quality and care in the public realm.

Each waste station includes receptacles for three streams of waste: trash, recycling, and compostable materials. The selection of receptacle products shall be reviewed and coordinated with the maintenance provider.

DESIGN CRITERIA

All receptacles shall be:

- Durable, resilient, and suited to hard use and cleaning.
- Easy and clean to use.
- High-capacity to minimize collection frequency.
- Side-opening and covered, for rain protection.
- Vandal resistant.

LOCATION CRITERIA

• On Type I, II and III Streets.

Waste stations on sidewalks shall be located near cross-walks where:

- They are visible to pedestrians.
- They will be well used.
- There is ample space and they do not compromise pedestrian flow or any other sidewalk function.
- They do not conflict with access to underground utilities.

LIGHTING STANDARDS

OVERVIEW

The primary goal of most exterior lighting systems is functional: to provide adequate light for safety and security. However, a comprehensive lighting design for Uptown Whittier must also establish a nighttime identity, provide visual information, facilitate vehicular and pedestrian wayfinding as well as simplify maintenance.

An important goal of the lighting system will be to provide consistency and unity throughout the site. The use of common effects, sources and equipment among separate areas will strengthen the visual identity Uptown Whittier. Establishing a hierarchy of light fixtures and lamp sources with an intentional approach to the color and quality of light is important to the cohesive nighttime identity of Uptown. The scale, form, overall style, proportion, color, source, and spacing of the proposed lighting fixtures should be compatible with the overall character of other site elements.

The lighting approach for Uptown Whittier consists of a hierarchy of three primary light types - a double decorative light, a single decorative light and a standard roadway light - and several specialized light types that may be used to augment these fixtures.

Where possible, existing light fixtures may be kept in place.

DESIGN GUIDELINES + CONSIDERATIONS

For safety and security, it is necessary to have a primary layer of the site lighting along the streetscape. This can be achieved by adequate horizontal luminance at the ground for navigation of roads and pathways, and adequate vertical luminance at surfaces such as buildings, people, and planting to provide visual context.

Light pollution reduction and dark sky measures are also an important design consideration. For example, the Backlight, Uplight, and Glare (BUG) ratings of exterior light fixtures shall meet the criteria established in the current California Green Building Code such as CALGreen. Exterior lighting controls which may include but is not limited to motion sensing and dimming capability shall also be considered to allow for energy savings as well as preservation of the night sky.





- Double Decorative Light



Double Decorative Lights provide lighting at intersections and mid-block crossings within the core area of Uptown Whittier. They are used to mark these key nodes and signal greater pedestrian interface with the roadway. These lights, in combination with the Single Decorative Lights should be spaced and designed to provide all necessary lighting for both the roadway and pedestrian zones of the streetscape.

GUIDELINES

- Located at intersections and mid-block
- Maximum spacing of 75', specific spacing to be determined by photometric study.
- Lights should be provided at bus stops and intersections of public streets and alleyways.

12 - Single Decorative Light



Single Decorative Lights provide lighting along the length of blocks within the core area of Uptown Whittier. These lights, in combination with the Double Decorative Lights should be spaced and designed to provide all necessary lighting for both the roadway and pedestrian zones of the streetscape.

3 - Standard Roadway Light



Standard Roadway Lights are to be used within the residential areas of Uptown Whittier, using existing light fixtures in place and augmenting these where necessary.

.4 - Festoon Lights



Festoon lighting may be used to mark heavily used pedestrian areas within the core area such as intersections, mid-block crossing and paseos. Festoon lights augment other street lights and are not used as a primary source of illumination.

5 - Other Specialized Lights



Other specialized lights may be incorporated into the streetscape to highlight key nodes, gateways or heavily used pedestrian areas. They should augment other street lights and should not be used as a primary source of illumination.

GUIDELINES

- Located along the length of Type I, II and III crossings on Type I, II and III Streets. Streets.
 - Maximum spacing of 75', specific spacing to be determined by photometric study.
 - Lights should be provided at bus stops and intersections of public streets and alleyways.

GUIDELINES

- Located on Residential Streets and alleyways.
- Maximum spacing of 75' on streets and 200' in alleyways, specific spacing to be determined by photometric study.
- Lights should be provided at bus stops and intersections of public streets and alleyways.

ART INSTALLATION STANDARDS

OVERVIEW

Public art is an important aspect of the Uptown Whittier streetscape design. Public art has the ability to unify Uptown Whittier with a distinct identify and distinguish both its core and gateways. At a pedestrian scale, it can provide visual interest for passersby. Public art can add interest and delight to a pedestrian's experience.

Public art may include special installations, murals, sculpture or enhanced streetscape elements. Public art may be permanent or temporary in nature.





PUBLIC ART









DESIGN CRITERIA

- Public art should be unique and specific to its site condition.
- Consideration should be given to incorporating art into otherwise standard street elements such as light poles, benches, trash receptacles, and utility boxes.
- Art can provide information, such as including maps and signage, or be educational in regards to the history and culture of Uptown Whittier. All installations do not need an educational mission, however—art can be playful.

- Public art should be located so as to be a pedestrian amenity. A piece can act as a focal point in a park or plaza or present a "surprise" along a pedestrian path that rewards the passerby with visual interest.
- Public art should be located on streets and in public spaces such as paseos, particularly at key points or intersections. It may also be located in areas where few people pass to create unique and special places for people to enjoy.
- Public art should be accessible to persons with disabilities and must not be placed in a way that compromises the clear path of travel. Art pieces may require detectable warning strips around the base of the art piece.

ACCESSIBILITY + LOADING STANDARDS

OVERVIEW

On-street accessible parking and loading zones for passengers, valet, rideshare services and commercial deliveries shall be located throughout the core area of Uptown Whittier, providing convenient access to the Uptown's buildings and open spaces. On-street parking will be supported by multispace meters within the core area.

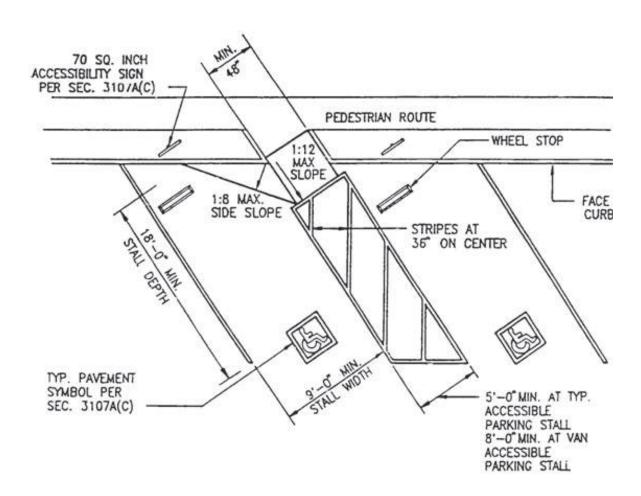
This section provides an overview of Uptown's approach and standard design of accessible parking and loading zones. The exact location and detailed design of specific zones shall be determined on a block by block basis and in coordination with applicable codes.



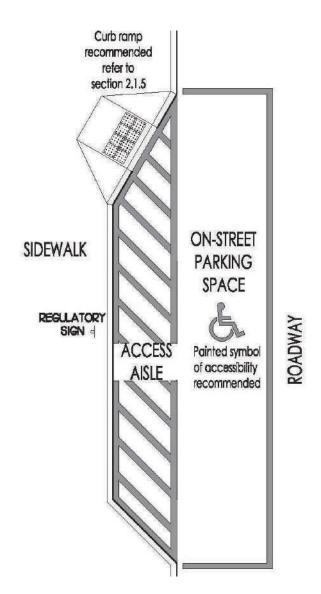


TYPF A1

ACCESSIBLE ON-STREET PARKING ZONE



DIAGONAL PARKING STALLS



Accessible parking stalls ensure convenient, equal parking access for drivers and passengers with a valid disabled parking permit.

LOCATION CRITERIA

On Type I Streets, generally accessible parking stalls may be located at the beginning of the block in parallel parking areas and mid-block in angled parking areas. Accessible parking should be studied on a case by case basis to see if off-street accessible parking may already meet or may be employed to meet applicable codes.

The diagrams provided are examples of potential design solutions. Consult applicable codes for current design requirements.

LOADING ZONES

TYPE A2 LOADING ZONES

Loading zones for passenger, valet, rideshare and commercial deliveries are curbside stalls, limited to 10 minute stops. Drivers must remain with the vehicle unless it is a commercial delivery. Loading zones may be converted from parking stalls on a scheduled basis.

DESIGN CRITERIA

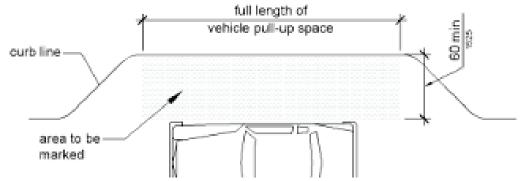
Per Typical Layout:

- 20' stall, adjacent sidewalk clear of objects
- 10' loading area at rear, with curb ramp

LOCATION CRITERIA

On Type I Streets, passenger loading zones shall be located at the end of the block face near intersections and within parallel parking zones to provide convenient access to building entrances on the block.









YPE A3 PARKING METERS





To ensure the availability of centrally-located on-street parking for potential customers and visitors rather than for business owners and employees, on-street parking spaces will be controlled by meters. The installation of parking meters will result in a quicker turnaround time for arriving/departing vehicles and will encourage employees wishing to park for a longer period of time to park in one of the nearby designated off-street parking lots.

LOCATION CRITERIA

On Type I, II and III Streets, multi space meters will be provided at the ends of all streets and midblock of each N-S block.

TYPE A4 REMOVABLE BOLLARDS







As a means to encourage and facilitate community and city events, removable bollards shall be implemented to control street access and provide a range of spacial considerations for a wide variety of community driven events. Bollards shall be lockable and adhere to a consistent design aesthetic.

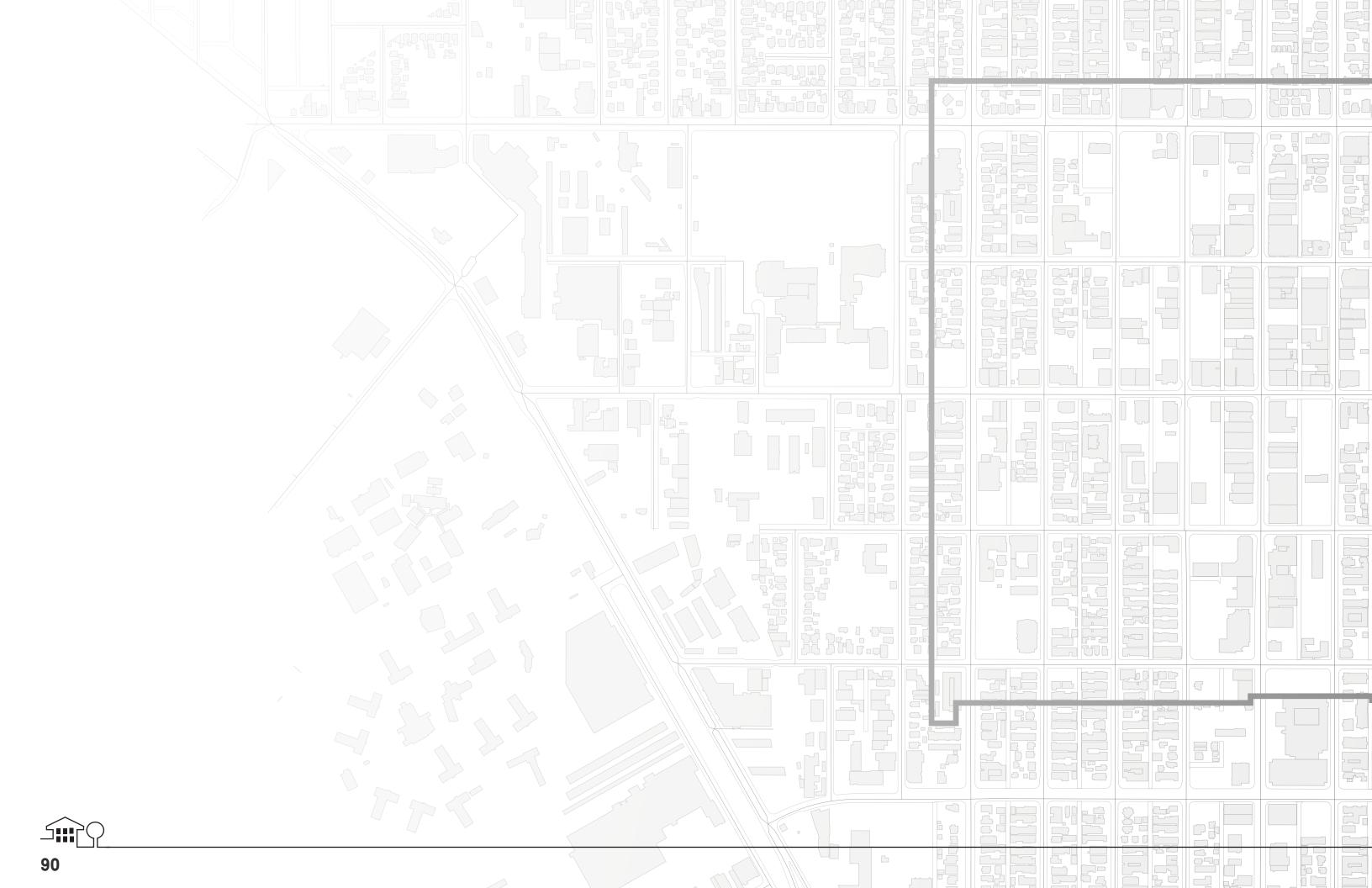
DESIGN CRITERIA

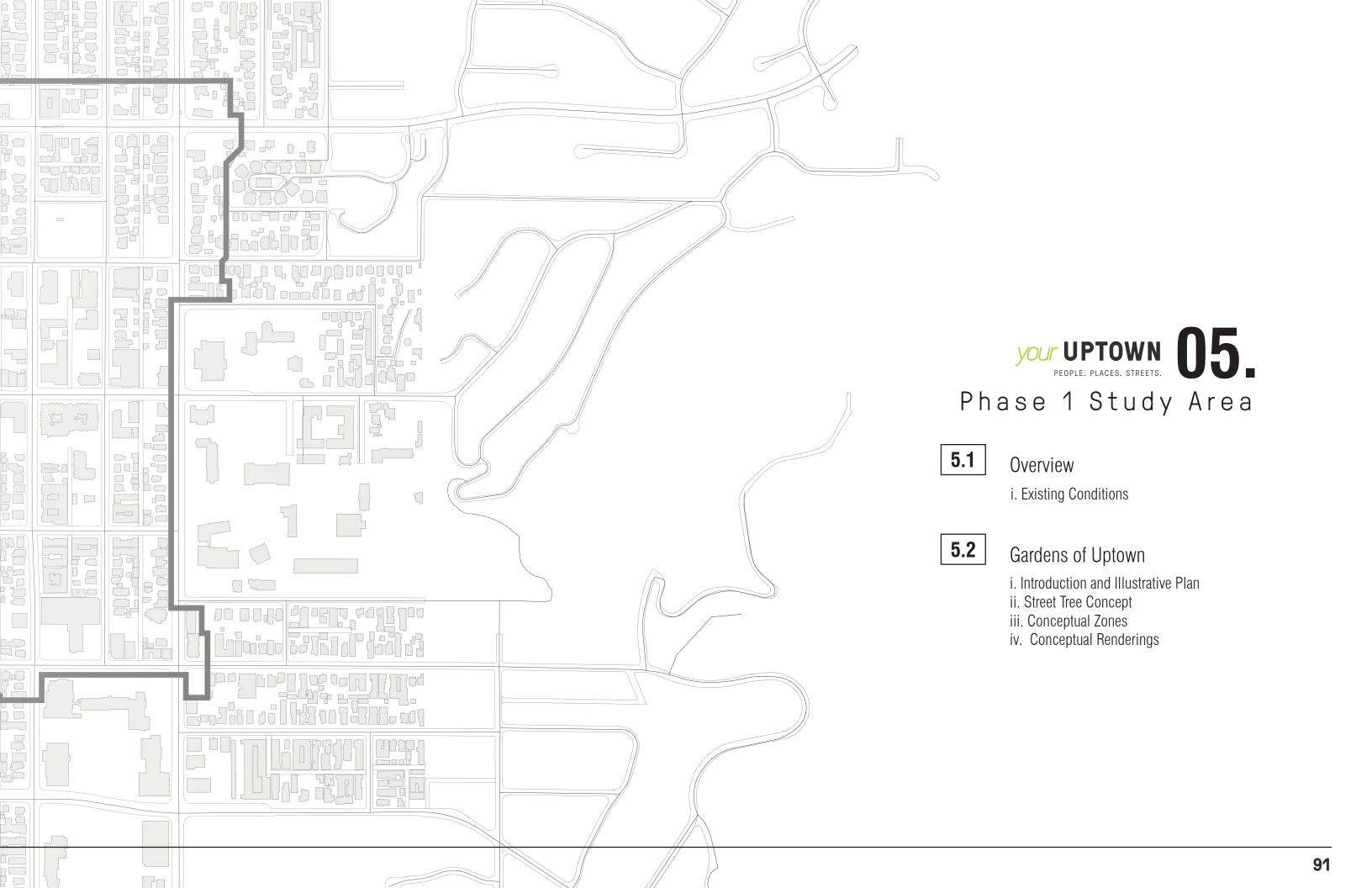
- Strong, consistent image: contemporary and timeless in style.
- Match furniture character of Uptown site furnishings

LOCATION CRITERIA

- Refer to diagram for location of removable bollards
- Bollards shall be located in areas that allow for different spacial types that can allow for a variety of small and larger event types.

The Streetscape Beautification Plan



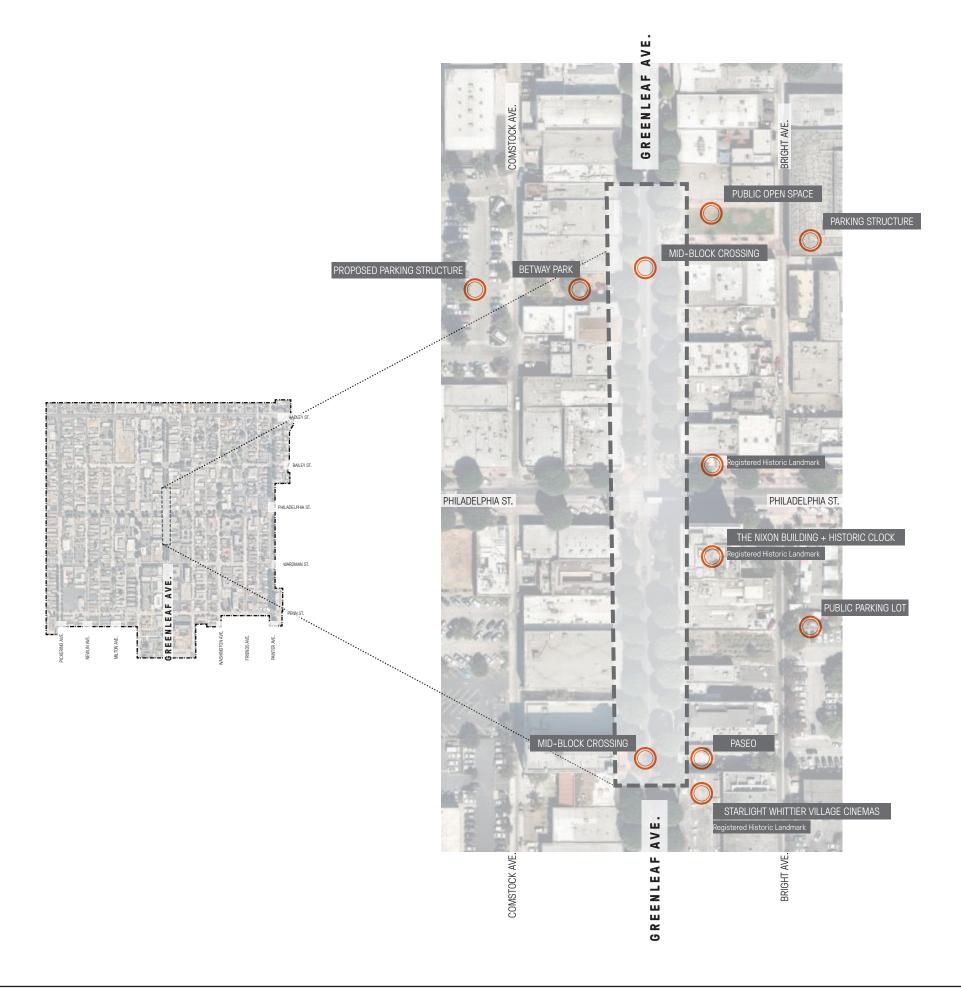


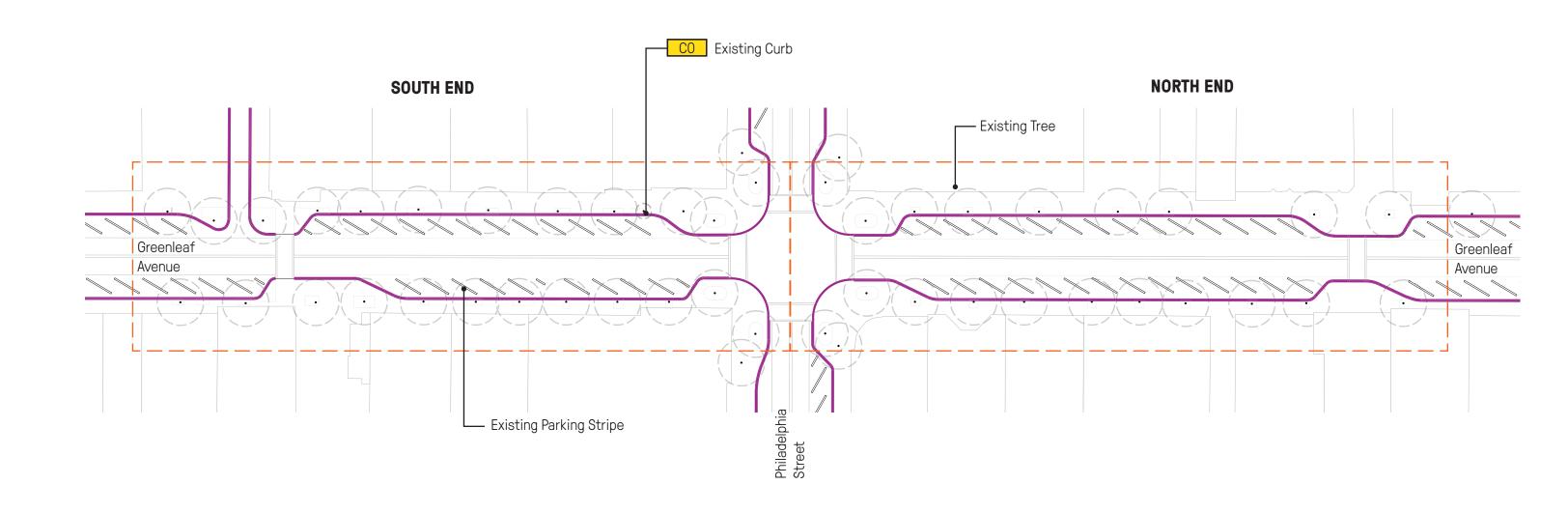
PHASE 1 STUDY AREA

OVERVIEW

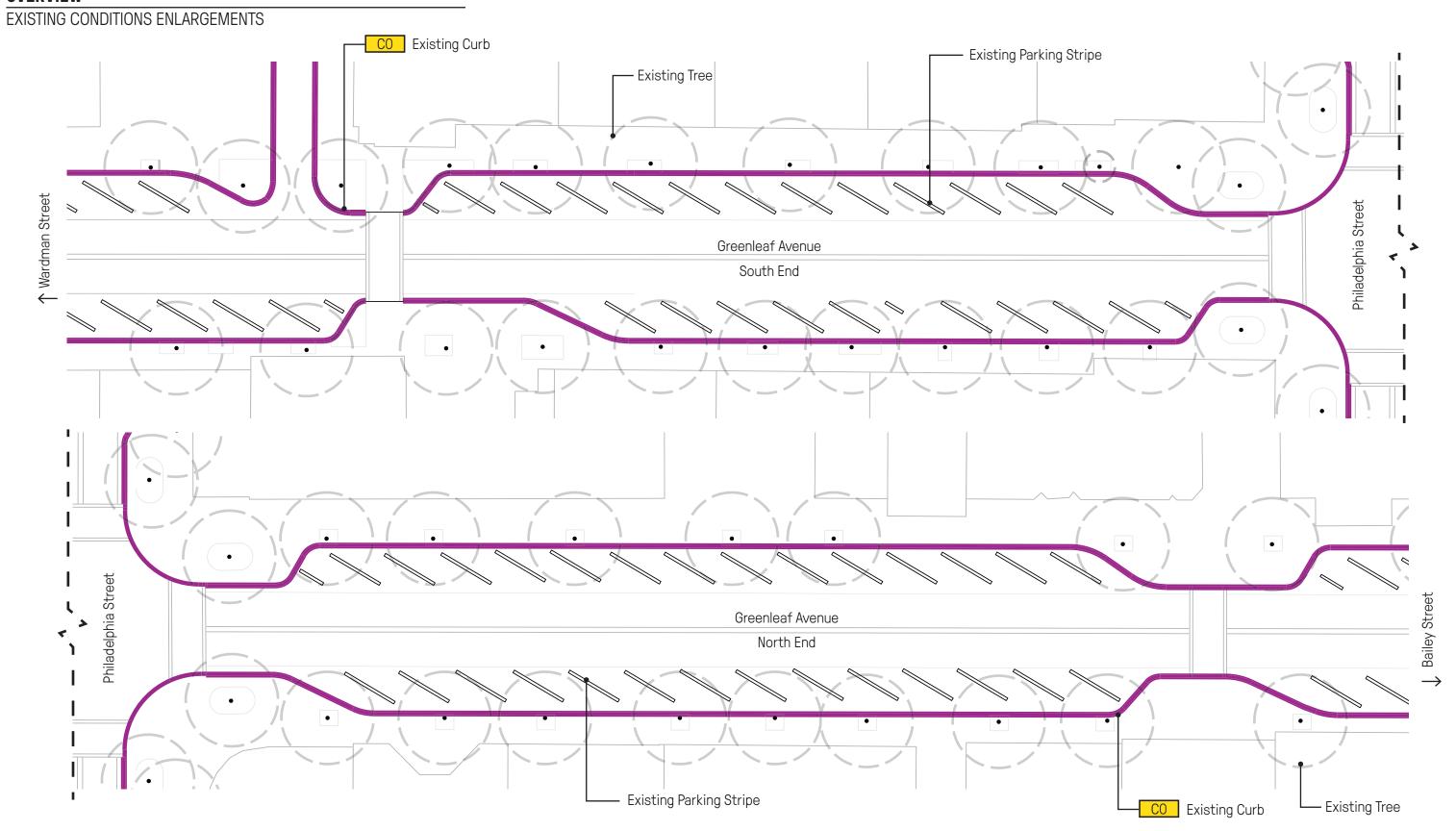
The Phase 1 Study Area looks at renovating one block within Uptown Whittier, focusing on the primary intersection of Grennleaf and Philadelphia and extending half a block to the north and south including the mid-block connections at either end. This one block contains most of the different options within the Uptown Whittier Streetscape Beautification Plan and serves to set the standard for future streetscape improvements.

The preferred concept for the one block study, Gardens of Uptown, combines new curb configurations at the end of each block with parallel parking as well as segments of preserved angled parking toward the center of each block. The plan balances a focus on gardens and gathering spaces with maintaining some of the existing conditions. The intervention focuses on intersections and nodes, keeping the simple concrete paving with some highlighted areas in brick. Three concepts illustrate the various patterns of ficus tree replacement to illustrate pros and cons of each method.





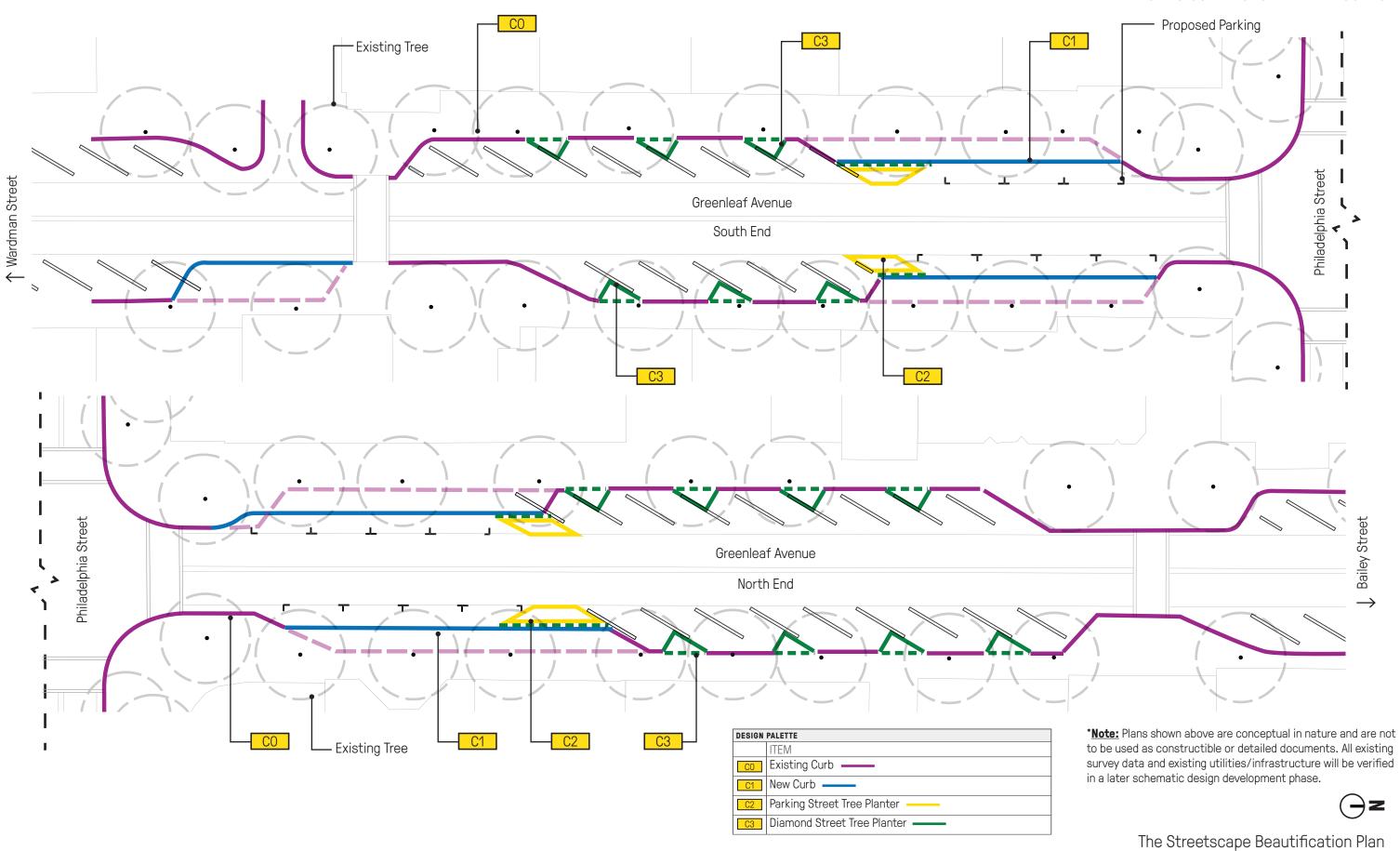






OVERVIEW

EXISTING CONDITIONS WITH NEW CURBS



Gardens of Uptown ONE-BLOCK CONCEPT

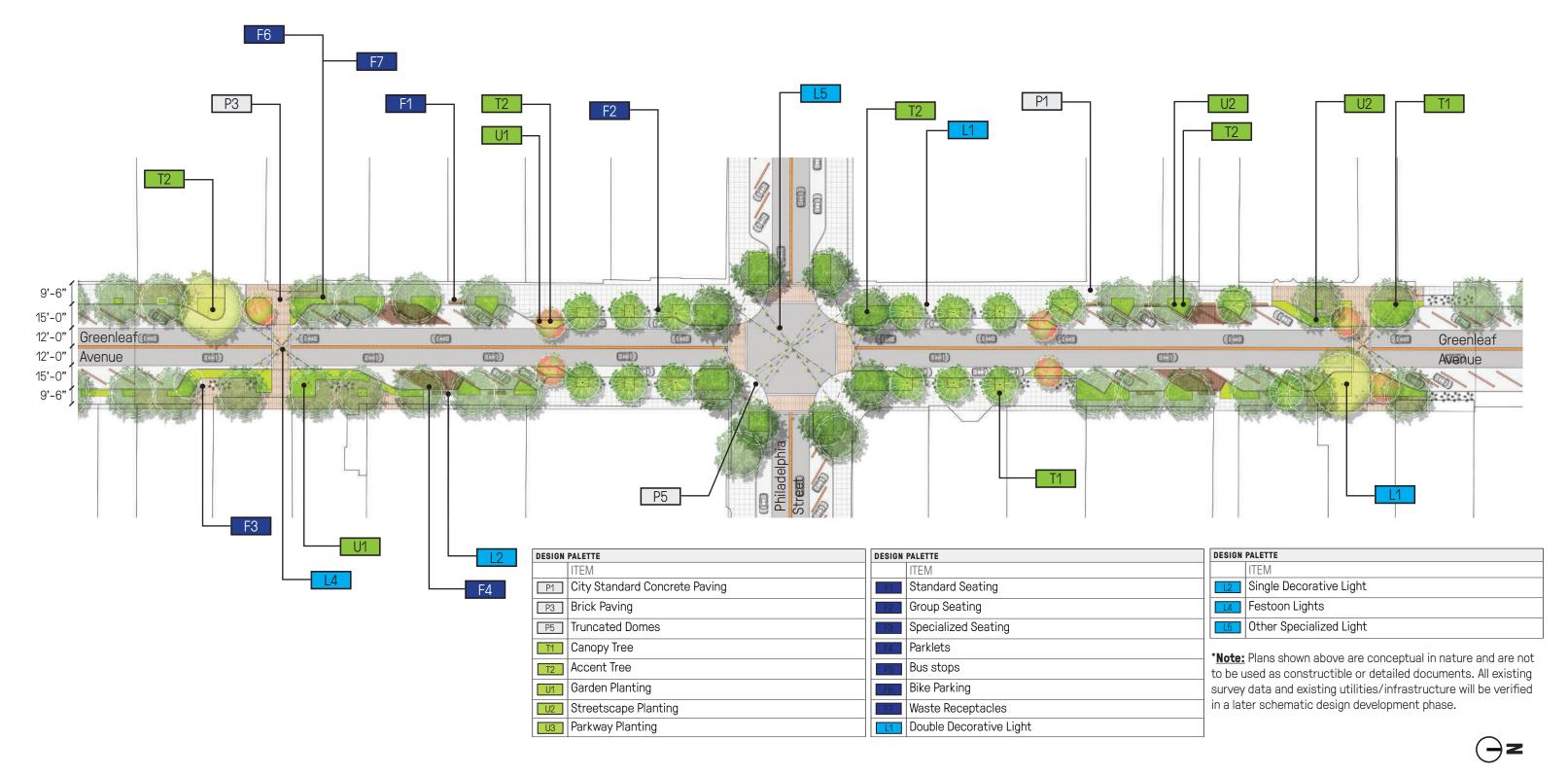
OVERVIEW

This concept keeps ficus trees in the angled parking area for the first phase of renovation, extending the existing tree pits into diamond planters. The areas with new curb extensions get new trees. This concept will need to revisit improvements as remaining ficus may damage new construction. Remaining ficus trees will need to be removed in future phases.

*Note: Plans shown above are conceptual in nature and are not to be used as constructible or detailed documents. All existing survey data and existing utilities/infrastructure will be verified in a later schematic design development phase.



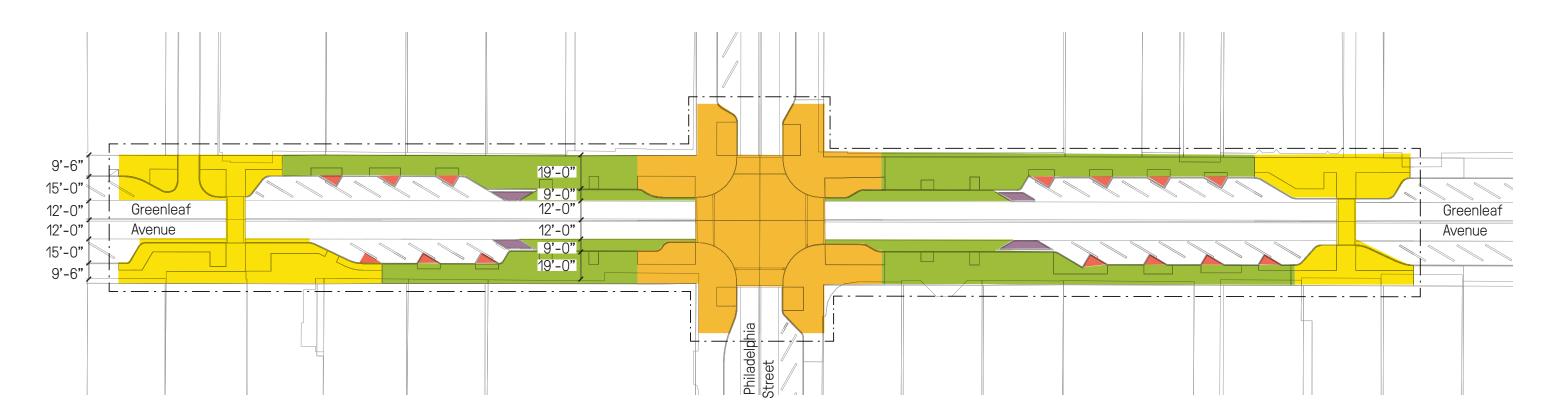




GARDENS OF UPTOWN

METRICS

Each streetscape option is measured in terms of changes to Trees and Planting, Gathering Space and Parking. The measurements are as follows:



TREES AND PLANTING

• Existing Trees: 44

• Existing Trees to remain: 22

New Trees: 31

• Total Trees : **53 7,586** SF Planting Areas

GATHERING SPACE

Parklets: 4

New SF of Pedestrian Zone : 31,226

• Existing SF of Pedestrian Zone : **30,999**

PARKING

Total Existing Parking: 64
Existing Angular Parking to Remain: 40
New Parallel Parking: 15

• Total Parking: **55**

ZONING LEGEND

Intersection

Streetscape

Diamond Planter

Parking Planter

Mid-block

Parklets

to be used as constructible or detailed documents. All existing survey data and existing utilities/infrastructure will be verified in a later schematic design development phase.

*Note: Plans shown above are conceptual in nature and are not





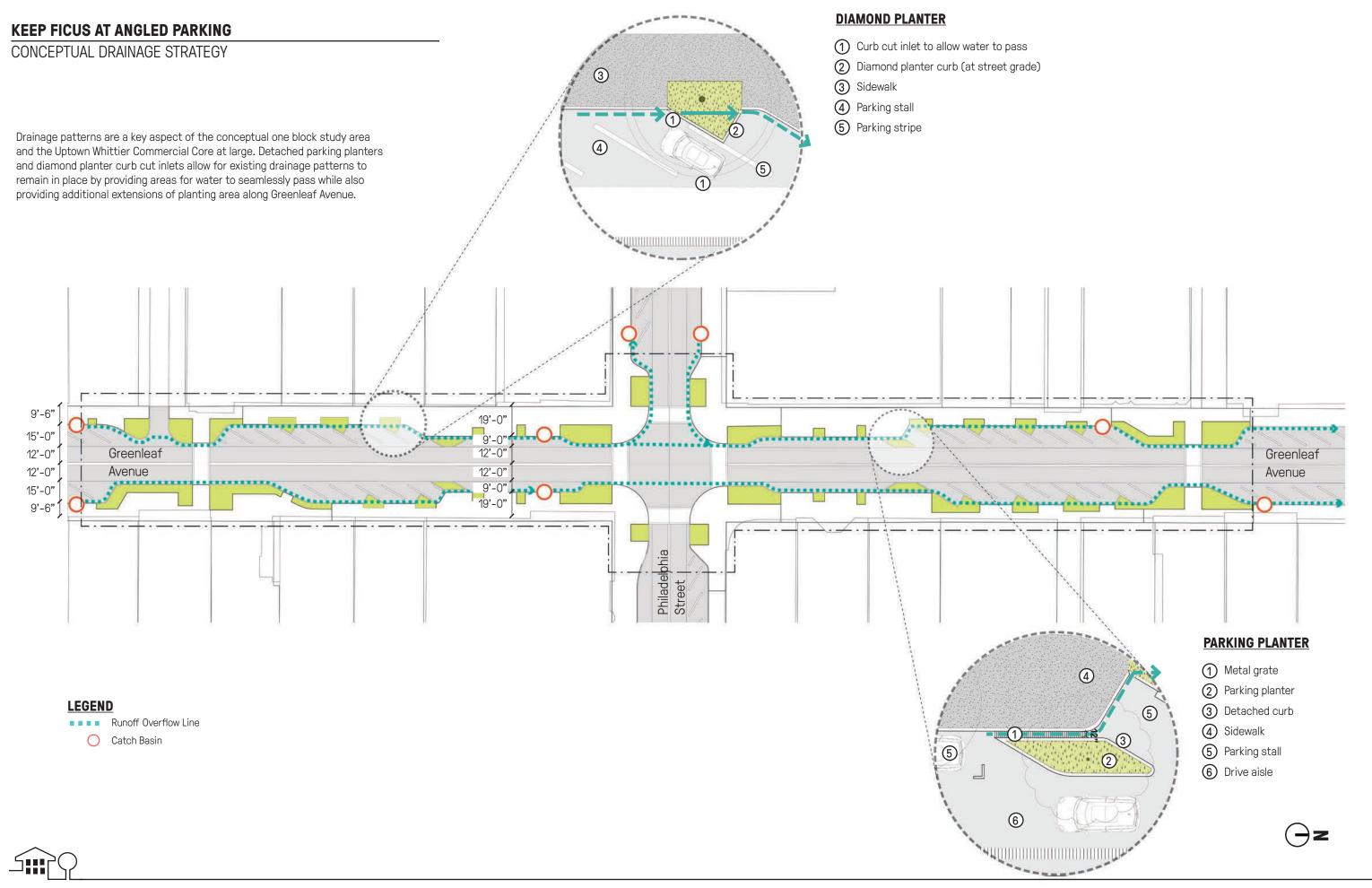
CONCEPT RENDERINGS





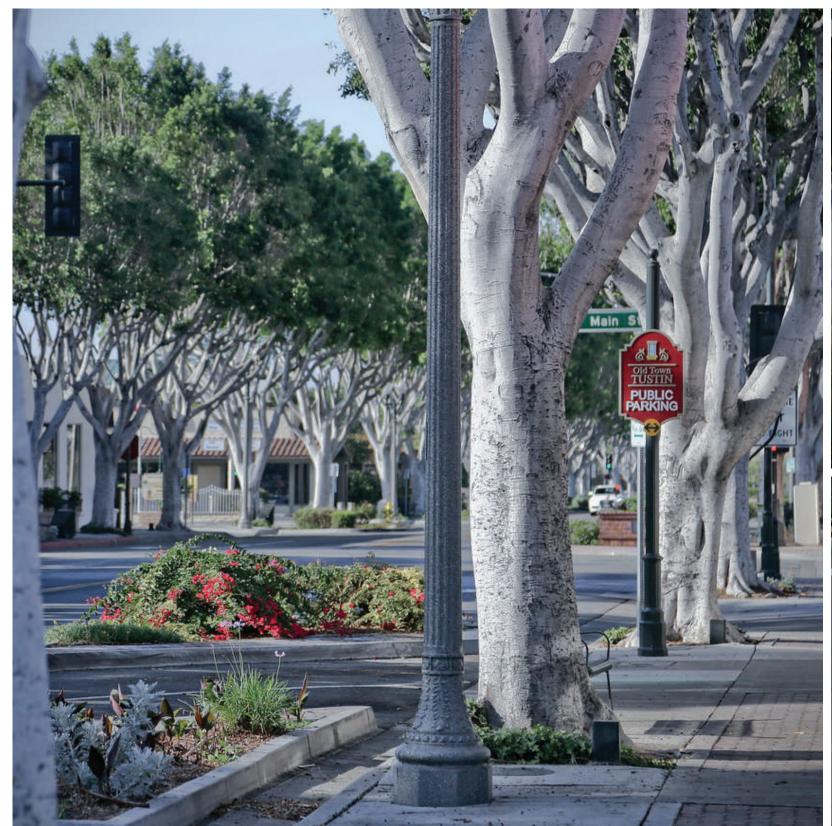






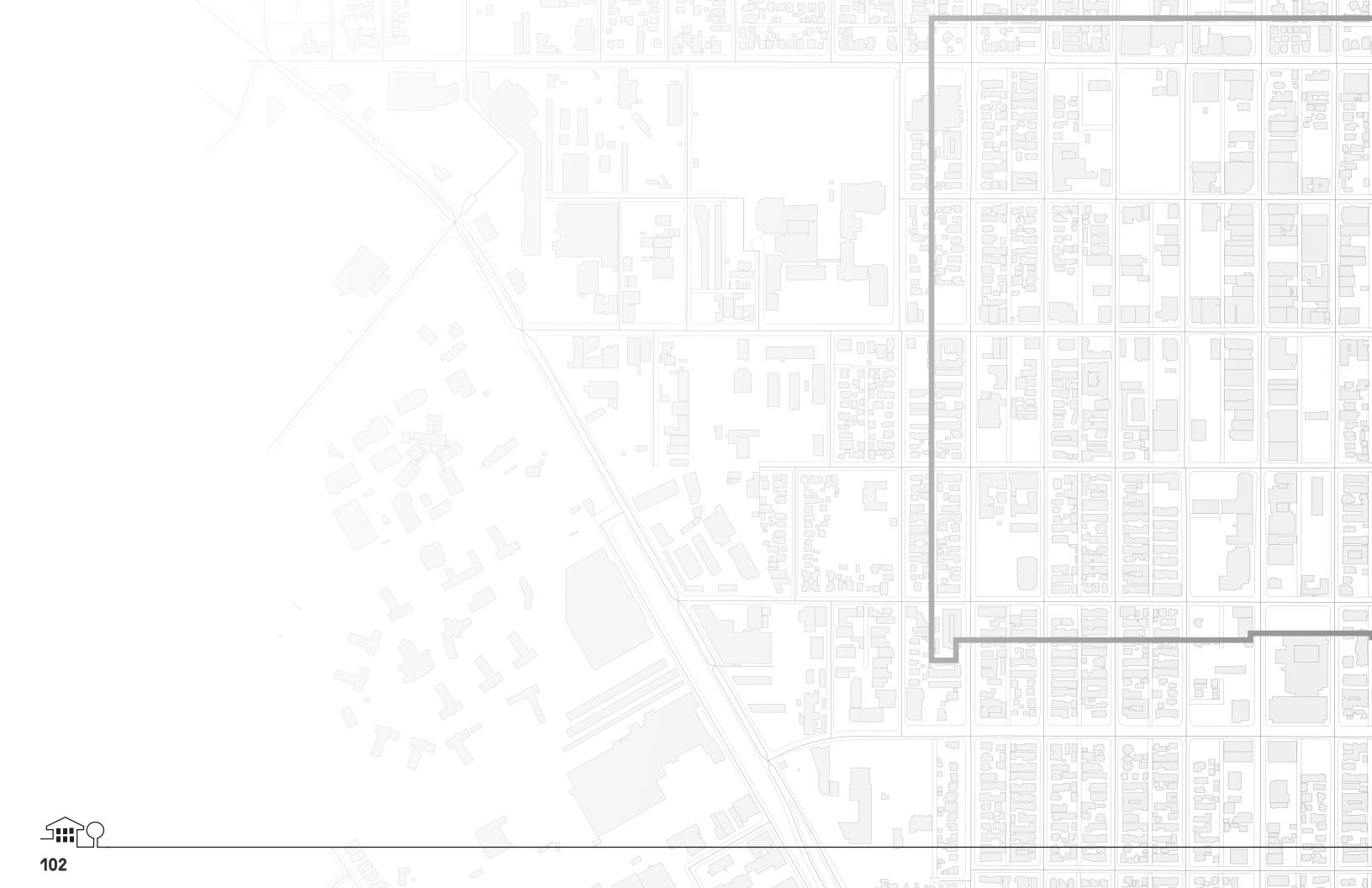
GARDENS OF UPTOWN - KEEP FICUS AT ANGLED PARKING

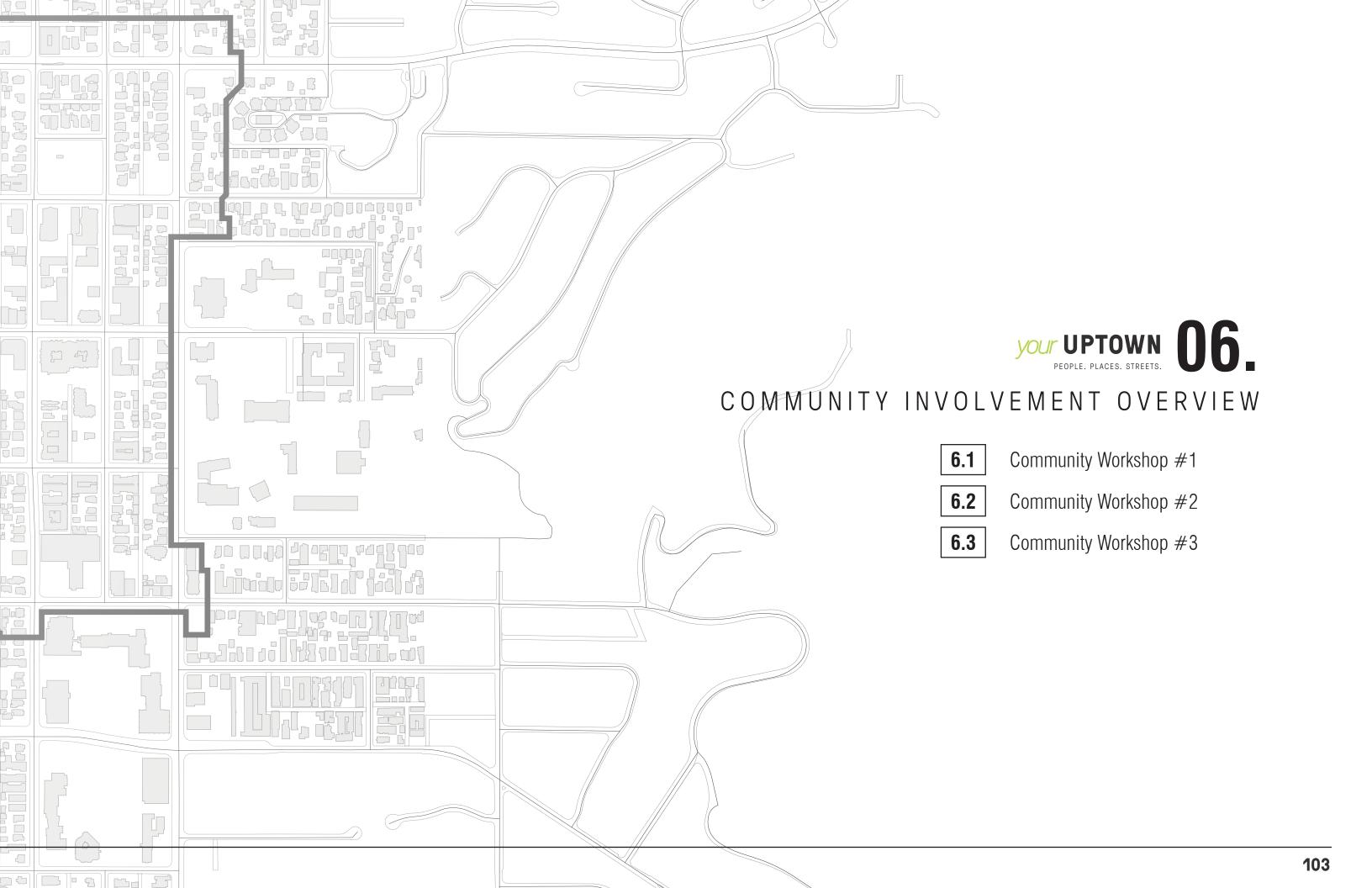
CONCEPTUAL DRAINAGE STRATEGY - IMAGERY











COMMUNITY WORKSHOP 1







PURPOSE

On Wednesday, November 30, 2016, the City of Whittier conducted the first community workshop for the Uptown Whittier Streetscape Beautification Plan. The workshop was held in the Nixon Plaza Building in Whittier, and the materials and content were presented by selected consultants SWA Group and Fuscoe Engineering.

Approximately 50 to 60 community members participated, and explored ideas, needs, and goals related to the Uptown streetscapes.

The purpose of the workshop was to:

- inform the community about the project and opportunities to participate
- solicit input on Uptown Whittier's unique qualities and needs related to the streetscape areas included in the project area
- understand the changes desired by the community
- present pros and cons related to existing conditions

RECAP & SUMMARY

The first community workshop was primarily facilitated by SWA Group, the planning and landscape architecture firm on the consultant team. The format was designed to give community members an equal voice in the visioning process, and to encourage participation from as many attendees as possible.

SWA Group gave presentations on topics relating to current site conditions, Uptown street trees, and streetscape experiences.

Attendees participated in a variety of ways, including: [1] pop quiz (written activity); attendees were asked to respond to 10 questions, recording each answer on a sticky note and posting each answer on exhibits posted on the wall.

[2] visual dot response activity for streetscape experiences; large format boards were placed around the room, each displaying several images that corresponded to a specific topic. Attendees were given 8 sticky dots and asked to place them on the experiences/photos that appealed to them the most.

[3] comment card written input (All input was carefully recorded and documented for use in guiding the visioning process)





MAJOR THEMES

OUTDOOR DINING / SIDEWALK CAFÉ SEATING / PARKLET PATIOS

Attendees reported most on visiting/enjoying restaurants and coffee shops. The enjoyment of outdoor seating and dining was very popular. Parklet patios and landscape enhancements along the streetscapes were mentioned as a top priority.

STREET TREES

Many commented that the streetscapes could be improved by planting tree species that keep the sidewalks cleaner, and allow for more light filtration. Evergreen and deciduous tree varieties were mentioned, along with a more unique street tree style and shape.

ARTFUL/FLEXIBLE GATHERING SPACES

The use of art and unique design in gathering spaces was commented on by many attendees. Art can take the form of sculpture, painted murals, installations, and temporary community spaces. Many people would like to see better programmed paseo and alley spaces, and more flexible entertainment/music and farmer's markets.

WALKABILITY

Many people commented on walkability as an important way of enhancing the streetscapes. Wider sidewalks, centralized parking garages, valet parking, enhanced street crossings and more efficient parking strategies were all mentioned as priorities.

SAFETY AND CLEANLINESS

Attendees felt that pedestrian safety was important. Some commented that street amenities and safe lighting enhancements would be a major improvement.



ACTIVITY #1 OVERVIEW

Activity 1 focused on getting direct feedback from the community on aspects of Uptown Whittier he or she valued while also beginning to listen to issues and concerns. Questions sought to target direct issues or values and greatly influenced the overall design direction. Community members expressed a great deal of pride for Uptown Whittier's culture and small town character and shared concerns of pedestrian safety and lack of formal gathering spaces along the streetscape.

What three qualities of Uptown Whittier do you value the most? 1.

restaurants

small town ambiance

community free parking food options architectural quality walkability family friendly location to other cities mix of land uses historic elements quaint shopping

Where do you go most frequently in Uptown Whittier?

tea church college bakerv the park theater work coffee shops bank restaurants everywhere

What place is most enjoyable to you in Uptown Whittier?

restaurants walking paseos shops coffee theater events historic buildings

What other downtowns/main streets do you enjoy visiting?



What qualities draw you to visit these other downtown/



small business

museums tech friendly

trees

What change could be made in Uptown Whittier that would bring vou here more?



On a scale of 1-5, how open are you to the replanting of Ficus trees with alternative species within Uptown Whittier?

least open to most open to replanting (7) replanting (27) no palms replanting (7)

replant with more appropriate trees moderately open to

Would you prefer to have a few more spaces to gather or a few 8. more spaces to park?

gathering spaces

What types of gathering spaces are missing in Uptown

children's playground plazas open green space live entertainment community garden music outdoor dining sidewalk cafes umbrellas outdoor seating amphitheater alleys community events

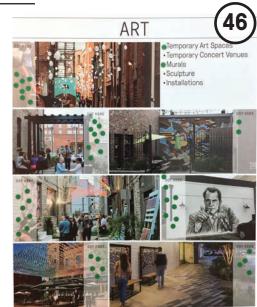
How many Ficus trees are currently planted in Uptown

401 to 500 too many 51 to 100 over 1,000 101 to 200 301 to 400

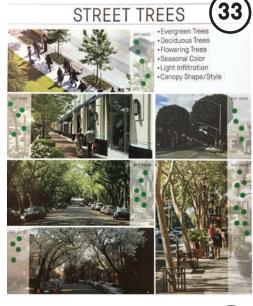
The Streetscape Beautification Plan

ACTIVITY #2 OVERVIEW

Activity 1 focused on getting direct feedback from the community on aspects of Uptown Whittier he or she valued while also beginning to listen to issues and concerns. Questions sought to target direct issues or values and greatly influenced the overall design direction. Community members expressed a great deal of pride for Uptown Whittier's culture and small town character and shared concerns of pedestrian safety and lack of formal gathering spaces along the streetscape.









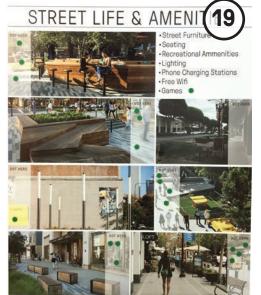






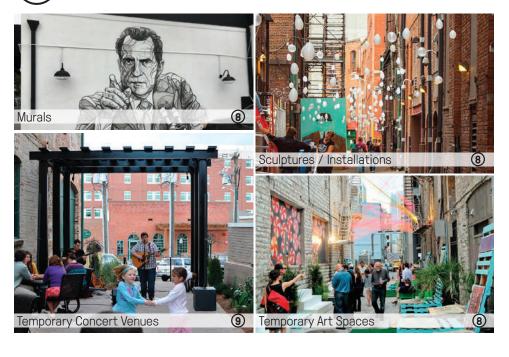








46) ART



(33) STREET TREES



(35) RETAIL/CAFE SEATING



(33) PARKLET PATIOS



- **46** ART
- 35 RETAIL/CAFE SEATING
- 33 PARKLET PATIOS
- 33 STREET TREES
- (31) Flexible Gathering Spaces
- 28 Green Infrastructure
- 24 Street Crossings
- 22) Parking
- 21) Signage & Wayfinding
- (19) Street Life & Amenities
- (16) Bicycle Facilities

Activity 1 focused on getting direct feedback from the community on aspects of Uptown Whittier he or she valued while also beginning to listen to issues and concerns. Questions sought to target direct issues or values and greatly influenced the overall design direction. Community members expressed a great deal of pride for Uptown Whittier's culture and small

COMMUNITY WORKSHOP 2









PURPOSE

On Wednesday, January 11, 2017, the City of Whittier conducted the second community workshop for the Uptown Whittier Streetscape Beautification Plan. The workshop was held in the Nixon Plaza Building in Whittier, and the materials and content were presented by selected consultants SWA Group and Fuscoe Engineering.

Approximately 75 community members participated, explored ideas, needs, and goals related to the Uptown streetscapes.

The purpose of the workshop was to:

- inform the community about the project and opportunities to participate
- solicit input on programming and project phasing for the streetscapes of Uptown Whittier

RECAP & SUMMARY

The second community workshop was primarily facilitated by SWA Group, the planning and landscape architecture firm on the consultant team. The format was designed to give community members an equal voice in the visioning process, and to encourage participation from as many attendees as possible.

SWA Group gave presentations on the following topics: community involvement updates, workshop #1 results, project guiding principles, Uptown Whittier programming, streetscape framework (existing and proposed), project phasing and implementation strategies, sample programming scenarios (making the programs real), and Ficus tree phasing feasibility and strategies.

Attendees participated in a variety of ways, including:

[1] Program your Uptown (group activity); Participants were grouped into tables of 6-8 persons per table, with each table having a kit of programming stickers and a 30"x72" base map board of the project focus area. Through collaborative thinking and discussion, the groups placed

program stickers accordingly throughout the base map. The groups reviewed concepts and discussed big ideas, and the scribe noted these on a large format post-it note. After the group exercise, each team reported back to the community on their big ideas and objectives for the streetscape beautification process.

[2] Ficus Phasing (dot polling); 4 large format boards were placed around the room, each displaying a strategy related to Ficus tree phasing strategies. Attendees were given 4 sticky dots and asked to place them on the strategy boards that they preferred the most.

[3] Comment card (written input)

All input was carefully recorded and documented for use in guiding the visioning process.

MAJOR THEMES

1. SAFETY ON THE STREET

A majority of the community members made reference to safety within Uptown Whittier. For the streetscape beautification effort, participants felt that better methods of lighting the streets, providing safe sidewalk zones and amenities (trash receptacles, benches, furnishings, bicycle racks, and more) for pedestrians would bring them to Uptown more often.

UPTOWN HISTORY APPLIED THROUGH ART & SIGNAGE

Participants felt that the culture of Uptown would be most enhanced through forms of art murals and installations, and an effective wayfinding and signage system. Many community members commented that this kind of treatment would enhance the streets, paseos, and alleys.

ENHANCE THE BUILT ENVIRONMENT

mind, much of the community was in favor of California native planting strategies being implemented in Uptown Whittier. In regards to the historic Ficus trees throughout Uptown, a dot polling activity revealed that participants felt that replacing Ficus trees was important, as well as increasing the maintenance of the trees. Some commented that the character of Uptown is defined by the Ficus trees, therefore removing all of them would not be effective in preserving historic character of the area. Generally, most participants were in favor of some change in the trees, however remained conscious of budgetary complications.

With issues of climate change and progressive methods of landscape in



ACTIVATE THE OUTSIDE

Much of the community would like to enjoy Uptown outside. All types of gathering spaces were commented on as a resource for the community. The streets of Uptown are considered a favorite for many community members, and they would like to see the street used more for the people by closing off the street to cars at least once a month. Parks, paseos, outdoor dining experiences were common on the list of wishes from participants. Several people would like to see the Alpha Beta site developed as a community resource/larger gathering space. Many people are aware of the valued spaces within Uptown, especially the paseos, and would like to see them programmed and designed to be more attractive places to gather or relax.

EXTEND UPTOWN

Several groups commented on the need to make Uptown accessible from different points of Whittier - an extension of the Greenway Trail into Uptown Whittier was a popular reference, and connections and access to Whittier College were also noted several times throughout the workshop. Many residents see Uptown as a potential destination for the city at large, and commented that the quality and quantity of their trips to Uptown varies because of safety concerns, parking restrictions, and issues with walkability to and from Uptown.



THE BOARD



ACTIVITY #1 OVERVIEW

Activity 1 focused on getting direct feedback from the community on aspects of Uptown Whittier he or she valued while also beginning to listen to issues and concerns. Questions sought to target direct issues or values and greatly influenced the overall design direction. Community members expressed a great deal of pride for Uptown Whittier's culture and small town character and shared concerns of pedestrian safety and lack of formal gathering spaces along the streetscape.

INSTRUCTIONS

- 1. Read instructions
- 2. Select volunteers (a reporter and a scribe)
- 3. Review themes and design kit handout
- 4. Program your uptown place stickers at points on the plan provided where you and your group would like to see that specific element present
- 5. Review concepts and discuss the big ideas
- Record your big ideas on the posters provided
- 7. Report back to the group on your big ideas

TABLE 1





BIG IDEAS













- Parking Garages
- Spaces- outdoor gathering (Rite-Aid, behind movie theatre, next to 99cent store)
- Bike racks/Parking
- Paved (multi-directional) walkways Bioswales

TABLE 2















BIG IDEAS

18

- 1 Block first; then roll out
- Parklet patios dining
- Clean trash cans
- Canopy trees
- Communal paseos
- Lighting
- Parking Garage / Angled Parking
- Murals & Installation

TABLE 3















BIG IDEAS

- Overhead outdoor lighting
- Connect bike paths to main streets
- Accent Philadelphia Street
- Cleanup alley ways

TABLE 4



BIG IDEAS

- Cars are not people
- No parking on Greenleaf
- Kids need play
- sports fields at Alpha Beta site
- No more empty lots!
- Don't forget Philly!
- Light the trees!
- More ART

TABLE 5



BIG IDEAS

- Creating paseos from abandoned areas
- Creating outdoor dining not tied to specific businesses
- Focus on installations in paseos
- Better lighting on Bailey and Greenleaf
- More accent trees and green space in general
- Adding permeable paving to parking areas
- Focal point at key intersections
- Overall improvements to sidewalks/infrastructure



TABLE 6



Table 6 BIG IDEAS

- Greenleaf blocked for cars 1x/month
- Enhanced lighting (main and side streets)
- Parking (adding vertically) /Lots of art everywhere
- Address tree issues + outside dining everywhere
- Storytelling and history throughout

TABLE 7



BIG IDEAS

- Focus Bright-Comstock and Wardman-Hadley
- Increase pedestrian traffic
- Reduce auto on Greenleaf (more parking)
- More paseos
- Outdoor dining with shade
- Attractive paving
- Space in front of garage as pedestrian focus

TABLE 8



BIG IDEAS

- Parallel parking to accommodate class 2 bike lanes
- Create access to Whittier College
- Enhance existing gallerias with lighting/urban play
- Canopy trees on GL/PH
- Benches/seating by theatre and by paseos
- Convert vacant city lots to public open space
- Angled parking along Comstock and Bright

TABLE 9



Uniform lighting

BIG IDEAS

- Grand Entrance+ branding (signage)
- Shifting focus outward towards Comstock and Bright
- Electric charging stations/Traffic calming
- Public restroom

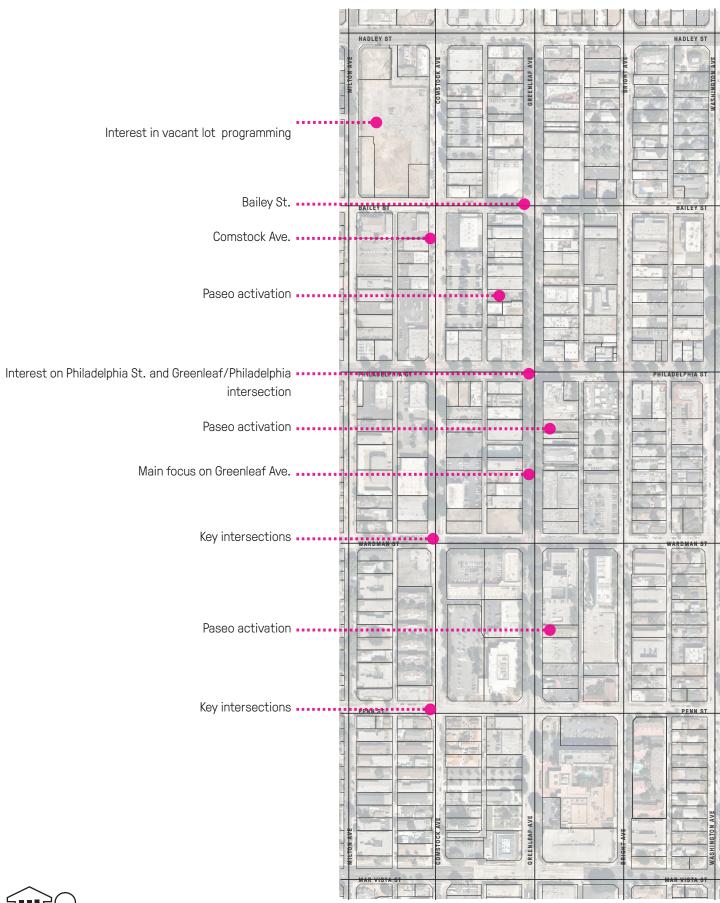
TABLE 10



BIG IDEAS

- Add canopy trees down Bright /Comstock
- Lighting needs to be added to lift atmosphere
- Add a play area for children (Ph and Bailey)
- Add lighting on main boulevards
- Want use of alleyways (want them lit)
- Outdoor dining at most Greenleaf and Bright
- Flexible gathering spaces at old Alpha Beta

The Sitreetscape Beautification Plan



MAJOR TAKE-AWAYS

KEY AREAS OF IMPLEMENTATION

- Greenleaf Ave./Philadelphia St. intersection
- Greenleaf Ave.
- Philadelphia St.
- Key intersections extending from Greenleaf Ave./Philadelphia St.
- Bright Ave. and Comstock Ave.
- Paseos on Greenleaf Avenue
- Alpha Beta site
- Connect Greenway Trail to Uptown Whittier

MOST PREFERRED STREETSCAPE PROGRAM ELEMENTS

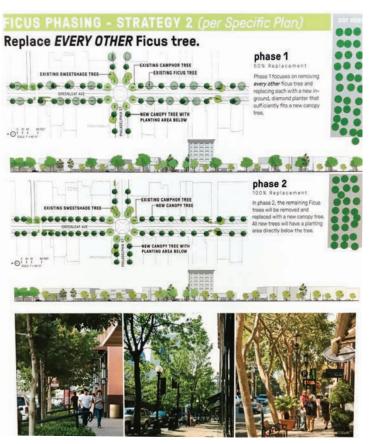
- Safe lighting and street amenities (Greenleaf Ave., Philadelphia St., intersections)
- Art murals, installations, and sculptures (paseos and major intersections)
- Specialty paving on sidewalks and major intersection crossings
- Wayfinding and signage with historic reference (major intersections and key entry points into Uptown Whittier)
- Canopy trees (on streets)
- Specimen and accent trees (in paseos, parks, and special use areas)
- 'Outside' dining and parklets (on Greenleaf Ave., Bright Ave., Comstock Ave., Philadelphia St.)
- Urban play elements and fountains (paseos and parks)
- Parking garages (consolidated parking strategy, "vertical parking")
- Permeable pavements (in existing parking lots)





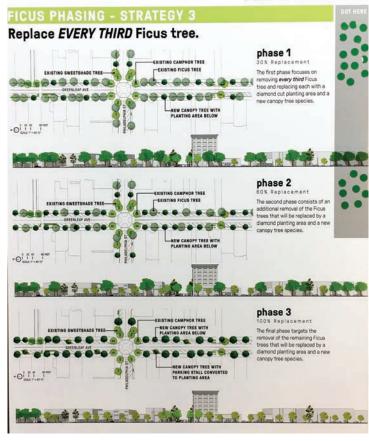
TRIM ON A YEARLY SCHEDULE





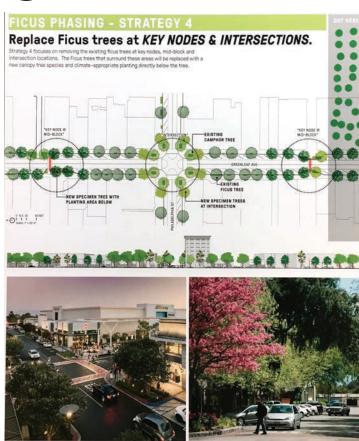
REPLACE EVERY OTHER FICUS TREE





03 REPLACE EVERY THIRD FICUS TREE





04 REPLACE TREES AT MID-BLOCKS + NODES

In Workshop #2, community members were given 7 sticker dots and instructed to place the dots on the option he or she most preferred. A large majority of voters were in favor of replacing every other ficus tree as they saw it to be the most efficient and quickest means of removing the ficus trees. However the option that received the second highest number of votes, focused on increasing the maintenance of the ficus trees, proving that other's prefer a method that allows certain ficus tree to remain.

COMMUNITY WORKSHOP 3







PURPOSE

On Wednesday, February 8, 2017, the City of Whittier conducted the third and final community workshop for the Uptown Whittier Streetscape Beautification Plan. The workshop was held in the Nixon Plaza Building in Whittier, and the materials and content were presented by selected consultants SWA Group and Fuscoe Engineering.

Approximately 35-40 community members participated and explored four different concepts related to the Uptown streetscapes.

The purpose of the workshop was to:

Inform the community about the project and opportunities to participate

Share design concepts, programmatic elements of the designs, and costs related to the concepts Solicit community input on each of the 4 streetscape concepts.

Conclude the community involvement portion of the Streetscape Beautification Plan and provide the

community members the next steps.

RECAP & SUMMARY

The third community workshop was primarily facilitated by SWA Group, the planning and landscape architecture firm on the consultant team. The format was designed to give community members an equal voice in the visioning process, and to encourage participation from as many attendees as possible.

SWA Group gave presentations on the following topics: community involvement updates, workshop #2 results, project guiding principles, streetscape framework (existing and proposed), design zone elements (kit of parts), and the 4 streetscape concepts.

Attendees participated in a variety of ways, including:

[1] Build your Uptown (dot polling): 5 large format boards were placed around the room, each displaying a specific

design element with an associated cost. Attendees were given 9 sticky dots, signifying the \$900,000 budget, and asked to place them on the design elements from the 4 concepts that he or she preferred most. The aim was to gather community input on the most preferred zone elements as a means to better understand how to apply these elements across Uptown Whittier.

[2] Discuss your Uptown (group format): Participants were grouped into tables of 6-8 persons per table to discuss the "pros" and "cons" of each of the 4 concept plans. Groups then recorded their input onto a large format board and reported back to the community on the "pros" and "cons".

[3] Comment card (written input)
All input was carefully recorded and documented for use in quiding the visioning process.

MAJOR THEMES

- 1. PREFERENCE FOR A BOLD STATEMENT
- 2. STREETS FOR THE PEOPLE
- 3 MEANINGFUL GATHERING SPACES
- 4. FLEXIBLE FRAMEWORK
- 5. SUPPORT FOR PARALLEL PARKING
- 6. FICUS TREE PHASING + NATIVE PLANTING



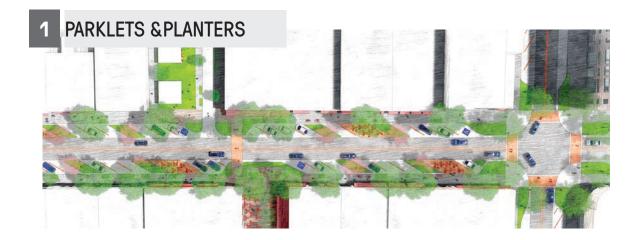


TABLE 1

PROS

Visual appearance

- Not practical
- · Needs more gathering space
- Street planters

TABLE 3

- Most street parking remains
- Modern appearance

CONS

- · Least amount of gatheringspace
- No bike lanes or electric charging ports

TABLE 4

PROS

- Visual appearance
- Angled parking good for traffic flow
- Lighting

CONS

Maintenance of parklets is a concern

TABLE 5

- Lose least amount of parking spaces
- Additional gathering areas gained with ficus tree removal

CONS

- Doesn't remove enough ficus
- Lacking sidewalk width
- Temporal quality of parklets / proximity to cars fumes



TABLE 1

PROS

- Budget efficient
- Practical location for new trees (widens sidewalk)
- Removes all ficus trees
- Parklets

CONS

 Needs more gathering space

TABLE 3

PROS

- Slight increases gathering space
- Least expensive

 No bike lanes or electric charging ports

TABLE 4 **PROS**

Native planting

- Removes ficus trees
- Diamond planters

CONS · Maintenance of parklets is a concern

TABLE 5

PROS

- Removes least amount of parking spaces
- Adds additional planting areas

CONS

- Not useful only visual
- Concerns from lacking sidewalk width and parklet locations



TABLE 1

PROS

- Mixed parallel and angled parking
- · Increases gathering space

CONS

 Ficus trees remain in many areas

TABLE 3

PROS

- Good sidewalk width variety
- Aesthetically pleasing
- More space for planting
- Keeps Ficus trees

CONS

· No bike lanes or electric charging ports

PROS

TABLE 4

- Mid-Block gathering space
- 50/50 Tree removal and replacement

CONS

- Parallel parking causes
- traffic delays
- · Table and chairs storage and possible theft.

TABLE 5

PROS

- Greater number of ficus trees removed
- · Native planting and increased sidewalk

- 2 different types of street parking
- · Some ficus trees remain



TABLE 1

- Eliminates all Ficus Trees
- Greatly increases gathering spaces
- Sidewalk Cafes
- Merchant option to utilize space for outdoor planters
- People focused, not automobile focused.

CONS

Less parking

TABLE 3

- Most pedestrian oriented
- Boldest statement
- Native plants

CONS

- Most expensive
- Potential traffic congestion due to parallel parking
- No bike lanes or bike amenities
- No electric car charging ports

TABLE 4

PROS

None listed

CONS

- Parallel parking slows traffic
- Removal of all the ficus trees at one time
- Streetscape improvement does not extend to Hadley Street and Wardman Street

TABLE 5

PROS

- Eliminates all Ficus Trees
- More walkable Increase in gathering spaces
- Native plantings Opens up street for people to gather

Drastically changes parking scheme

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ACTIVITY #2 OVERVIEW

Many of the community members selections reflected an attitude that focuses development within one key block of Uptown Whittier. Members preferred design elements that came from Option 3 and several from Option 4 and included:

- Extensions of sidewalk space
- Implementation of parallel parking
- Phasing strategies of ficus tree removal to occur overtime
- Extensions of gathering spaces achieved by extending sidewalk widths both at the mid-block and along several sections of the sidewalk as well as through parklet spaces.

| | Streetscape | Planter | Mid-Block | Parklet | Parallel | |
|---------------------|-------------|-----------|-----------|-----------|-----------|-----|
| | Z2 | Z3 | Z4 | Z5 | Z6 | |
| Parklets & Planter | 6 | 0 | 12 | 17 | n/a | 35 |
| Diamonds Of Uptown | 15 | 12 | 7 | 37 | n/a | 71 |
| Gardens Of Uptown | 22 | 29 | 1 | 17 | 36 | 105 |
| Gathering In Uptown | 22 | 25 | 34 | n/a | 35 | 116 |
| TOTAL VOTE | S 43 | 66 | 54 | 54 | 71 | |

NOTE: Numbers in red represent similar conditions in zone not portrayed and not included in zone totals





ZONE 2 STREETSCAPE







PARKING PLANTER



OPTION 2

















\$83,000

emove Ficus Trees frim All Existing Trees

New Sidewalk Paving

New Mid-Block Crosswall

New Tree & Shrub Planting New Site Furnishings















\$44,000 22 @ \$2,000 EACH

Planting Curb & Gutter







(34)







6



ZONE 6







PARKING PLANTER Option 3











MID-BLOCK Option 4





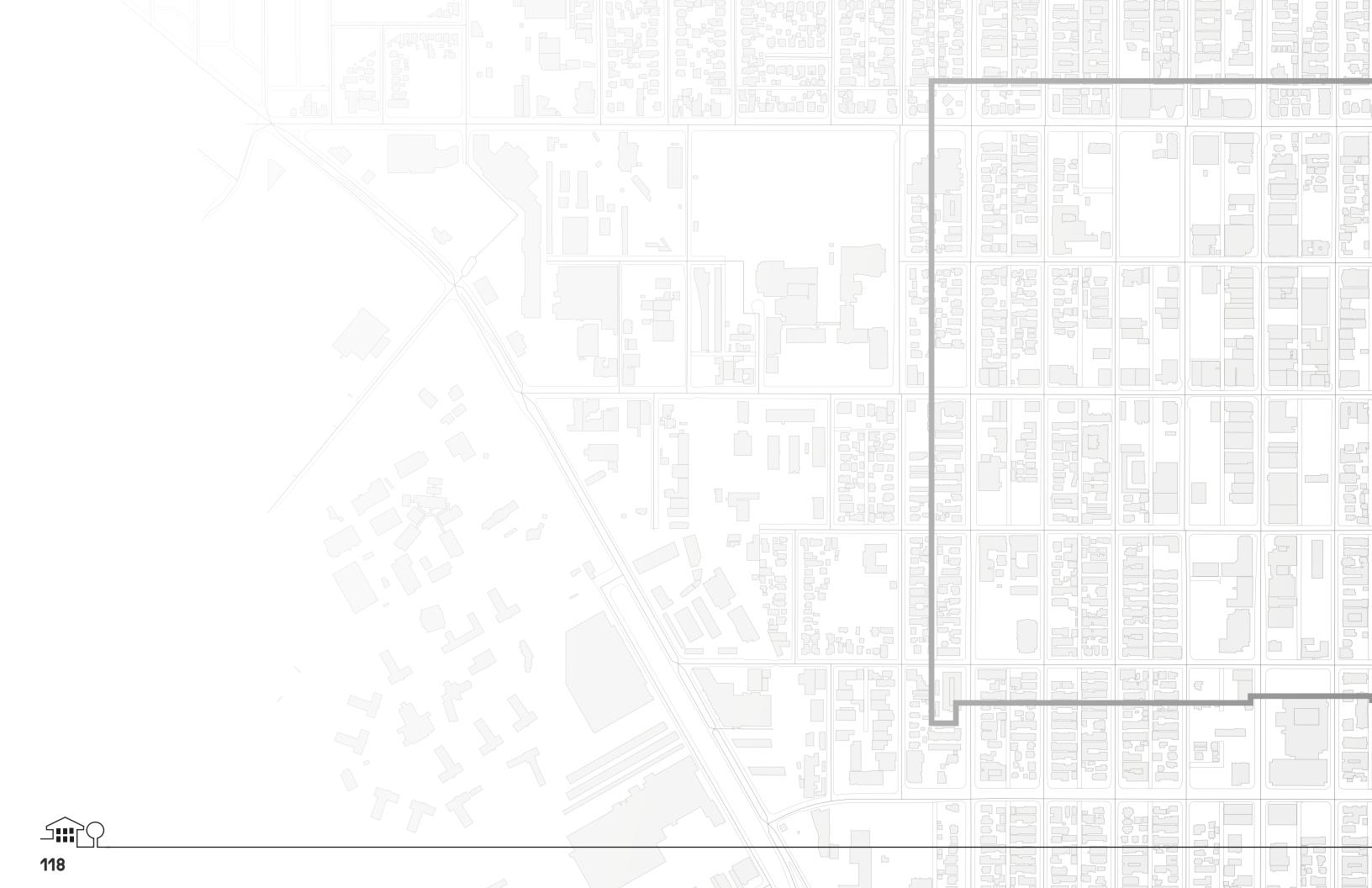


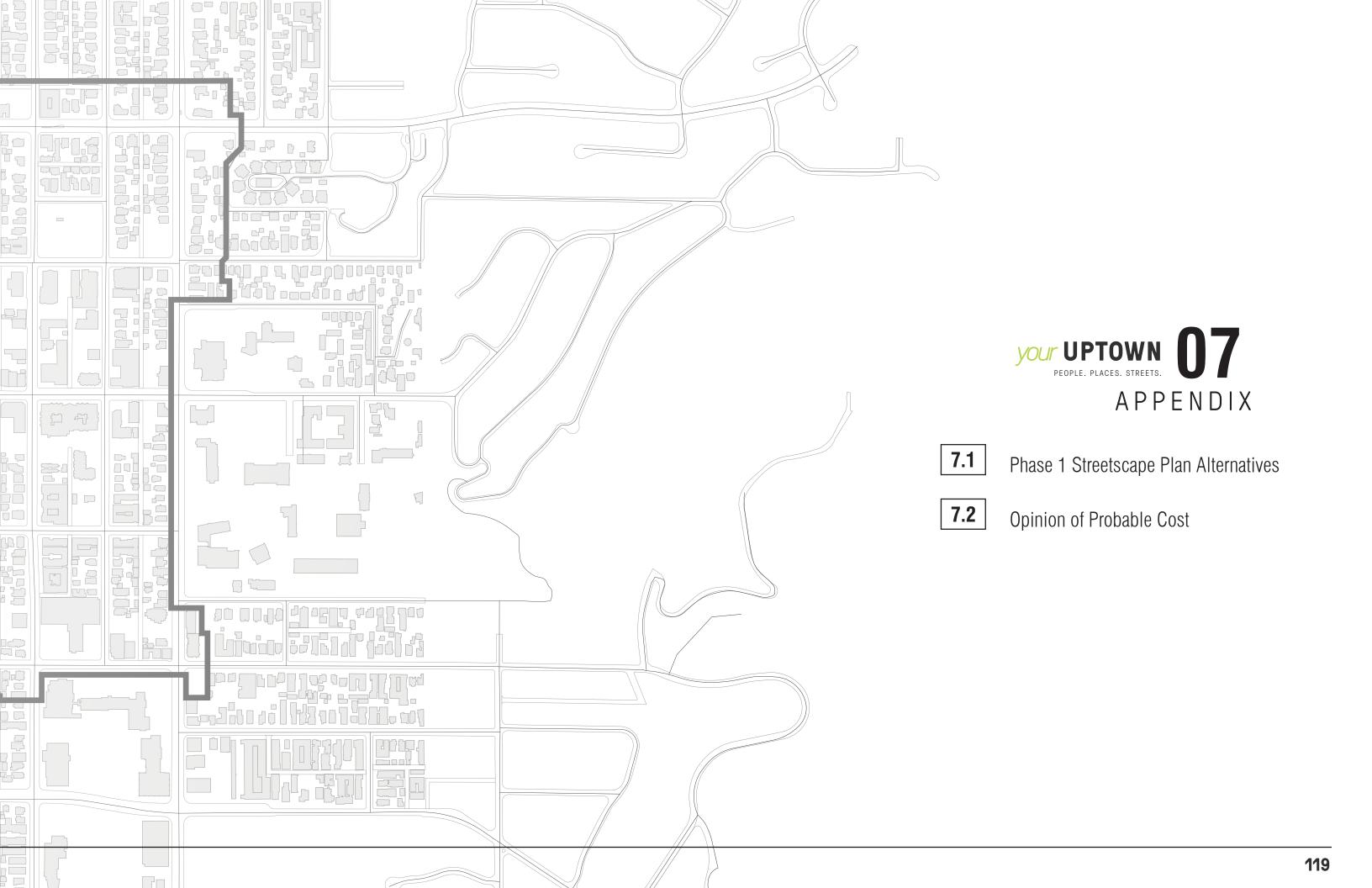


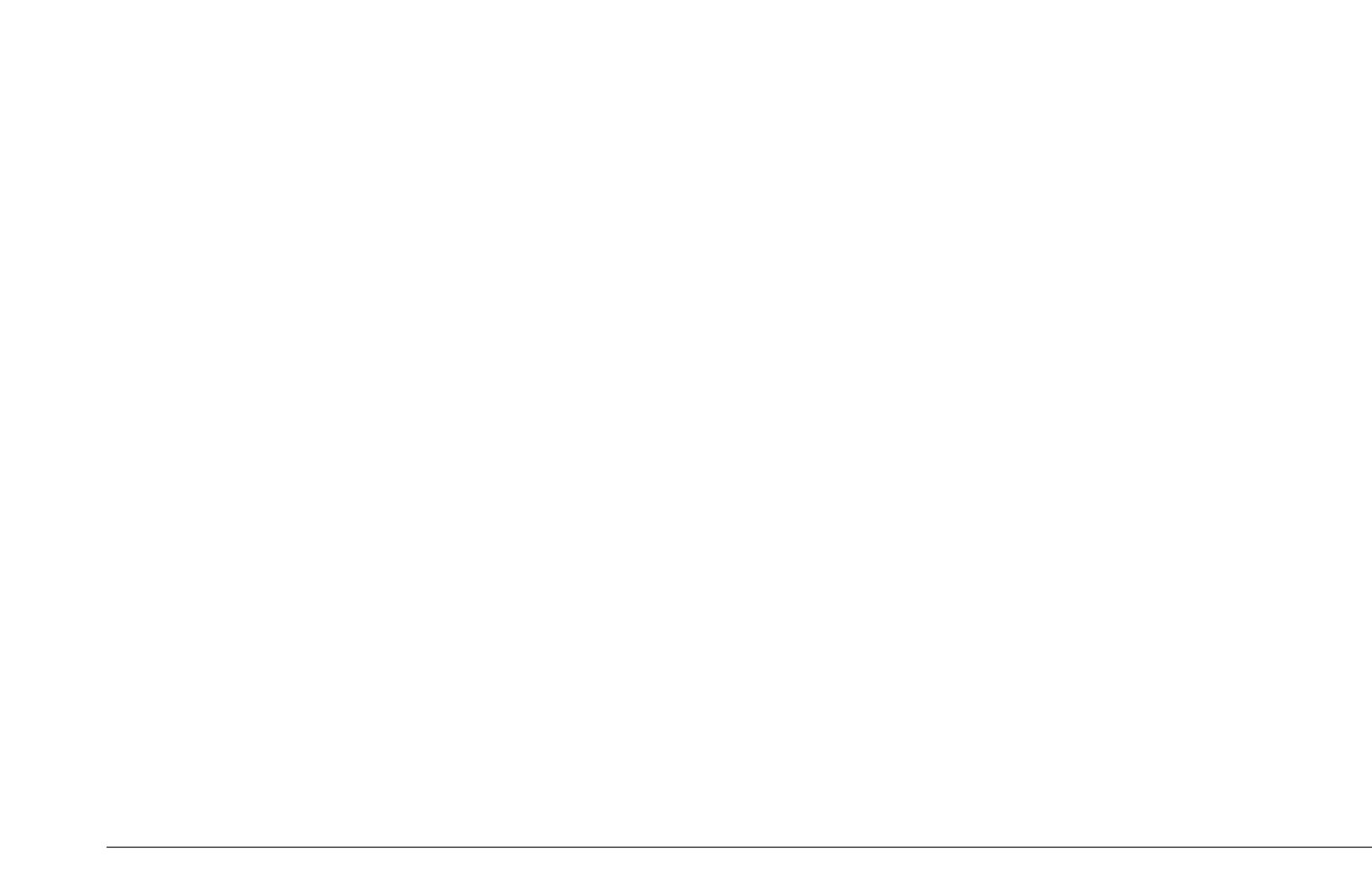










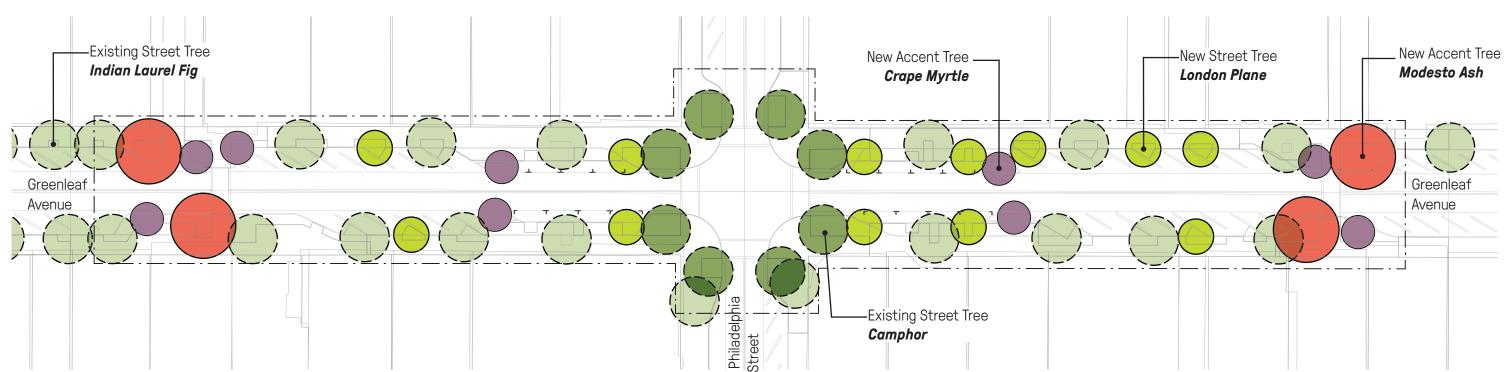


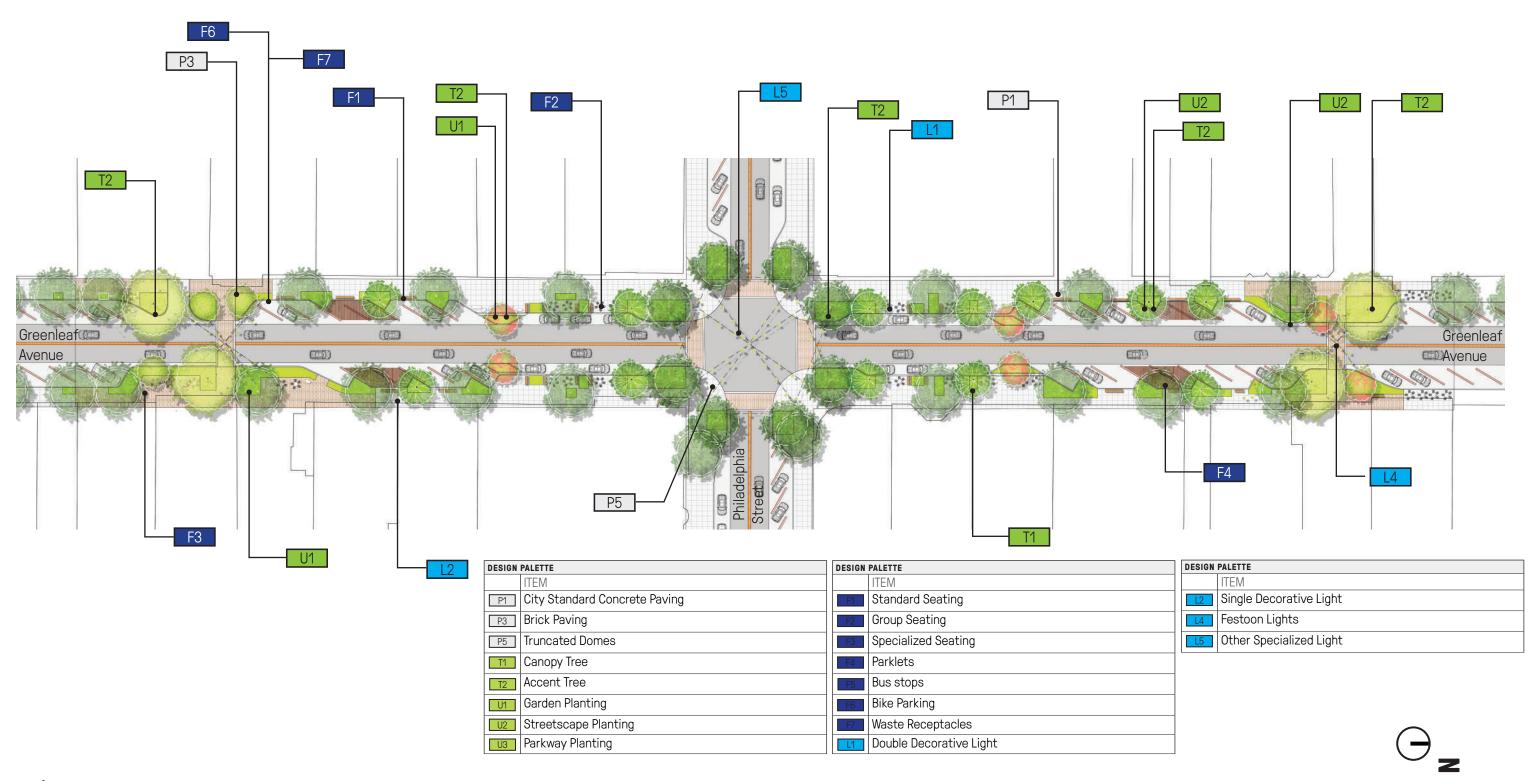
Gardens of Uptown KEEP EVERY OTHER FICUS

OVERVIEW

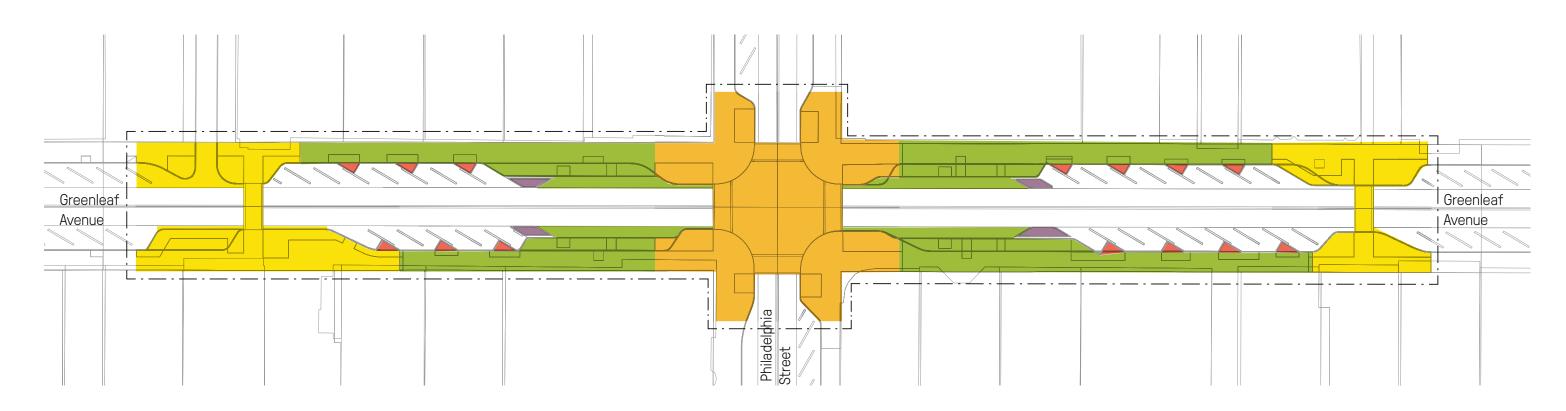
This concept replaces every other ficus tree along the length of the block, extending the existing tree pits into diamond planters where needed and providing space for existing trees within the extended curb areas as well as additional tree pits for future trees as needed. This plan does result in some irregular tree pit placement in the extended curb areas. This concept will need to revisit improvements as remaining ficus may damage new construction. Remaining ficus trees will need to be removed in future phases.







Each streetscape option is measured in terms of changes to Trees and Planting, Gathering Space and Parking. The measurements are as follows:



TREES AND PLANTING

- Existing Trees: 44Existing Trees to remain: 24
- New Trees : **25**
- Total Trees: 49
- **7,844** SF Planting Areas

GATHERING SPACE

- Parklets: 4
- New SF of Pedestrian Zone : 30,850
- Existing SF of Pedestrian Zone : **30,999**

PARKING

- Total Existing Parking: 64
 Existing Angular Parking to Remain: 40
 New Parallel Parking: 15
- Total Parking: **55**

Intersection Streetscape Diamond Planter Parking Planter

ZONING LEGEND

Mid-block Parklets

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GARDENS OF UPTOWN - KEEP EVERY OTHER FICUS

CONCEPT RENDERINGS







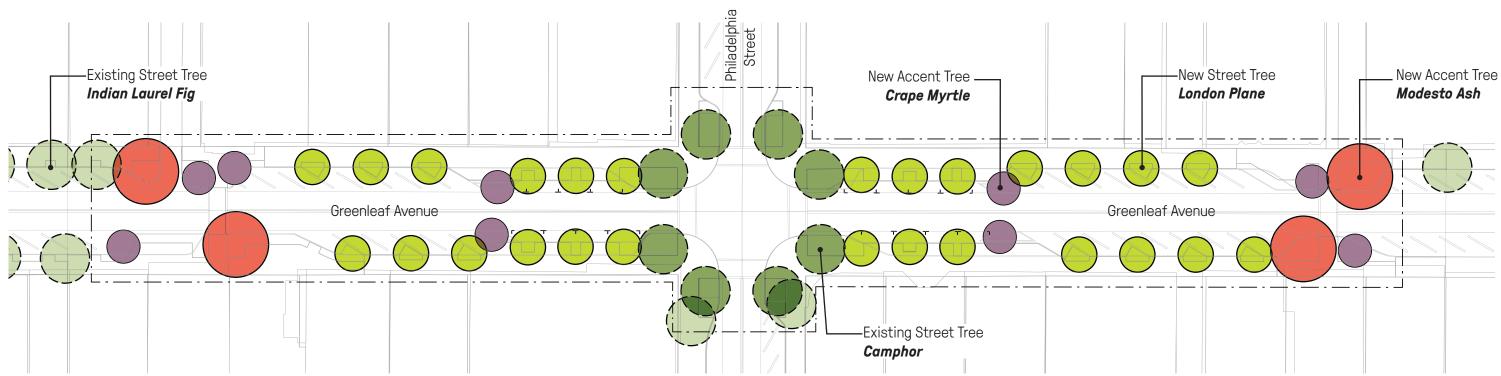


Gardens of Uptown **REMOVE ALL FICUS**

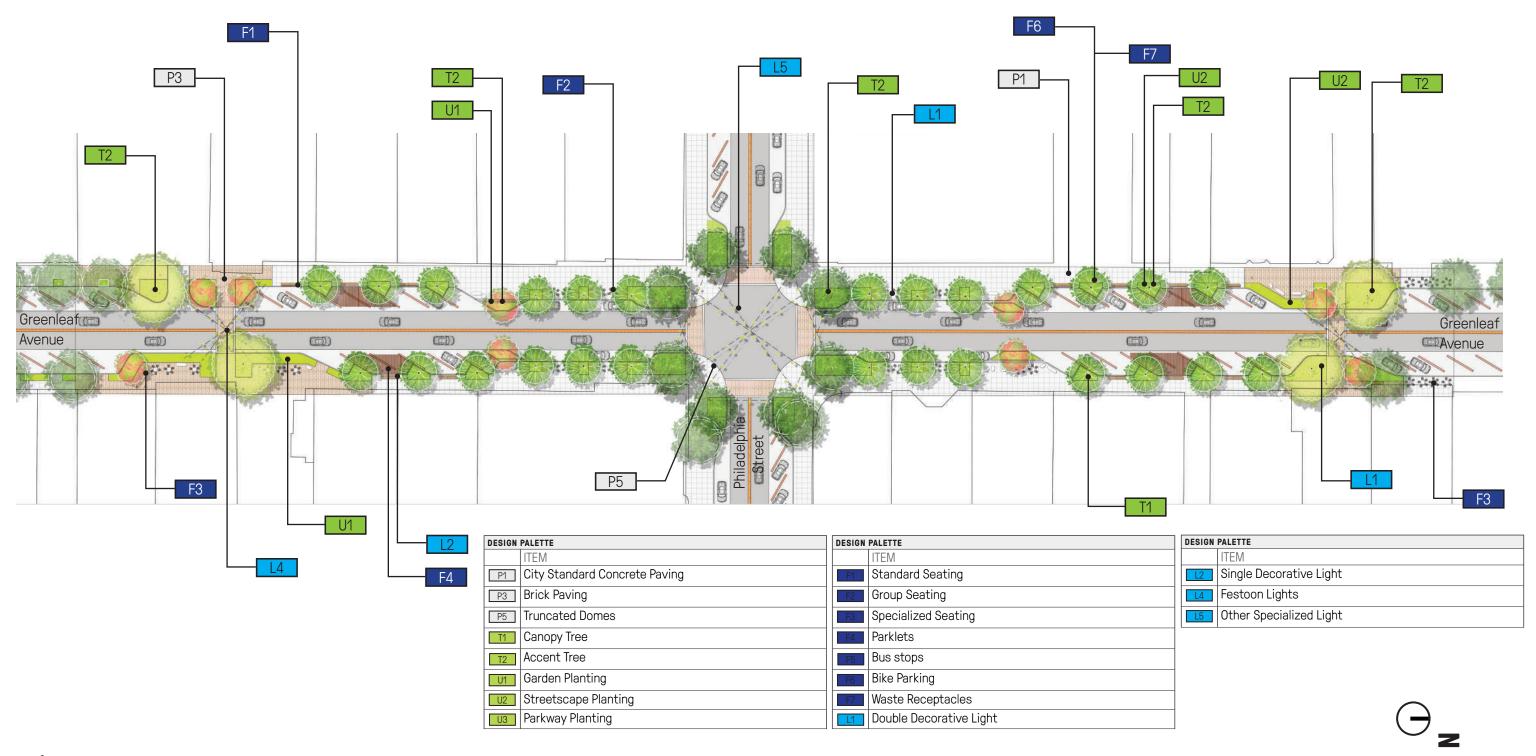
OVERVIEW

This concept replaces every ficus tree along the length of the block and provides all new trees. This concept will not need to revisit improvements for many years as all construction will be new enabling the proper planting of all street trees.



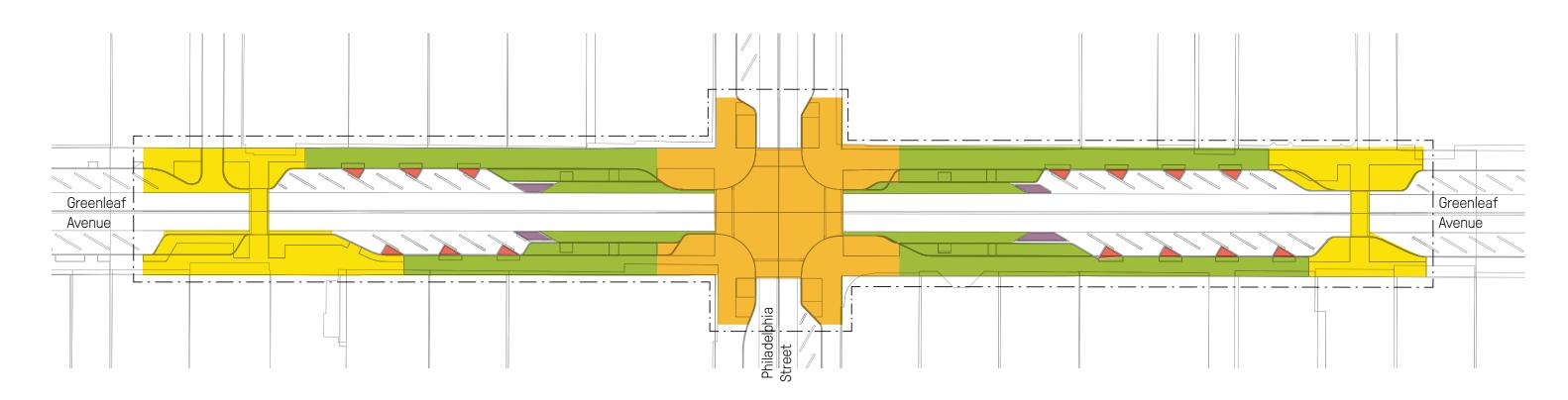


CONCEPT PLAN





Each streetscape option is measured in terms of changes to Trees and Planting, Gathering Space and Parking. The measurements are as follows:



TREES AND PLANTING

- Existing Trees: 44Existing Trees to remain: 9
- New Trees : 39
- Total Trees: 48
- **7,011** SF Planting Areas

GATHERING SPACE

- Parklets: 4
- New SF of Pedestrian Zone: **31,696**
- Existing SF of Pedestrian Zone : **30,999**

PARKING

- Total Existing Parking: 64
 Existing Angular Parking to Remain: 40
 New Parallel Parking: 15
- Total Parking: **55**

ZONING LEGEND



Streetscape

Diamond Planter

Parking Planter

Mid-block

Parklets

GARDENS OF UPTOWN - REMOVE ALL FICUS

CONCEPT RENDERINGS









OPTION COMPARISON

KEEP FICUS AT ANGLED PARKING

TREES AND PLANTING

• Existing Trees to remain: 22

- New Trees : 31
- Total Trees : **53** • **7,586** SF Planting Areas

GATHERING SPACE

- Parklets: 4
- New SF of Pedestrian Zone: **31,226**

PARKING

- Existing Angular Parking to Remain: 40New Parallel Parking: 15
- Total Parking : **55**



KEEP EVERY OTHER FICUS

TREES AND PLANTING

- Existing Trees to remain: 24
- New Trees : **25** Total Trees: 49
- **7,844** SF Planting Areas

GATHERING SPACE

- Parklets: 4
- New SF of Pedestrian Zone: **30,850**

PARKING

- Existing Angular Parking to Remain: 40New Parallel Parking: 15
- Total Parking: 55



REMOVE ALL FICUS

TREES AND PLANTING

- Existing Trees to remain : 9
- New Trees : 39 Total Trees: 48
- **7,011** SF Planting Areas

GATHERING SPACE

- Parklets: 4
- New SF of Pedestrian Zone: **31,696**

PARKING

- Existing Angular Parking to Remain: 40New Parallel Parking: 15
- Total Parking : **55**



The Streetscape Beautification Plan

OPTION COMPARISON

